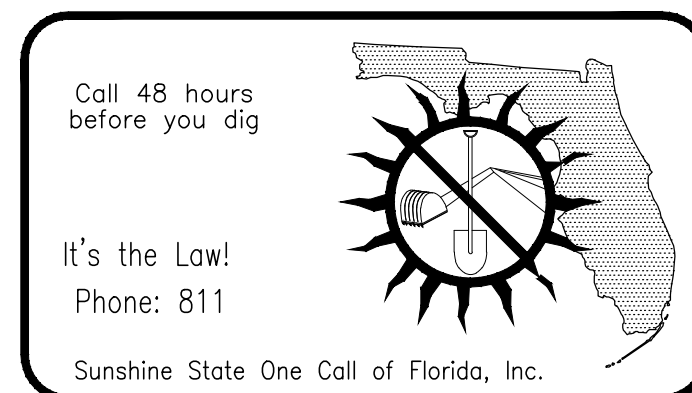
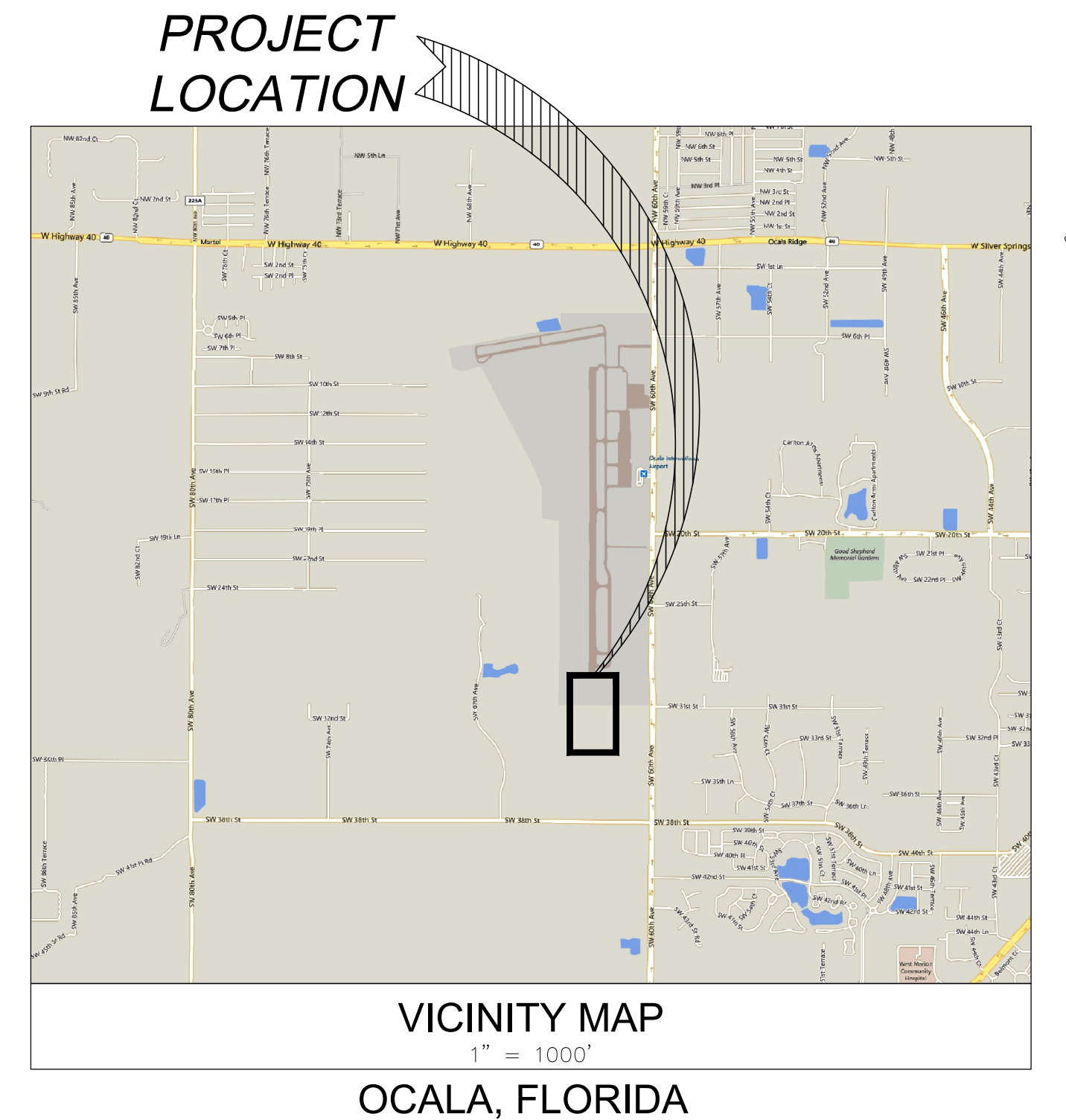
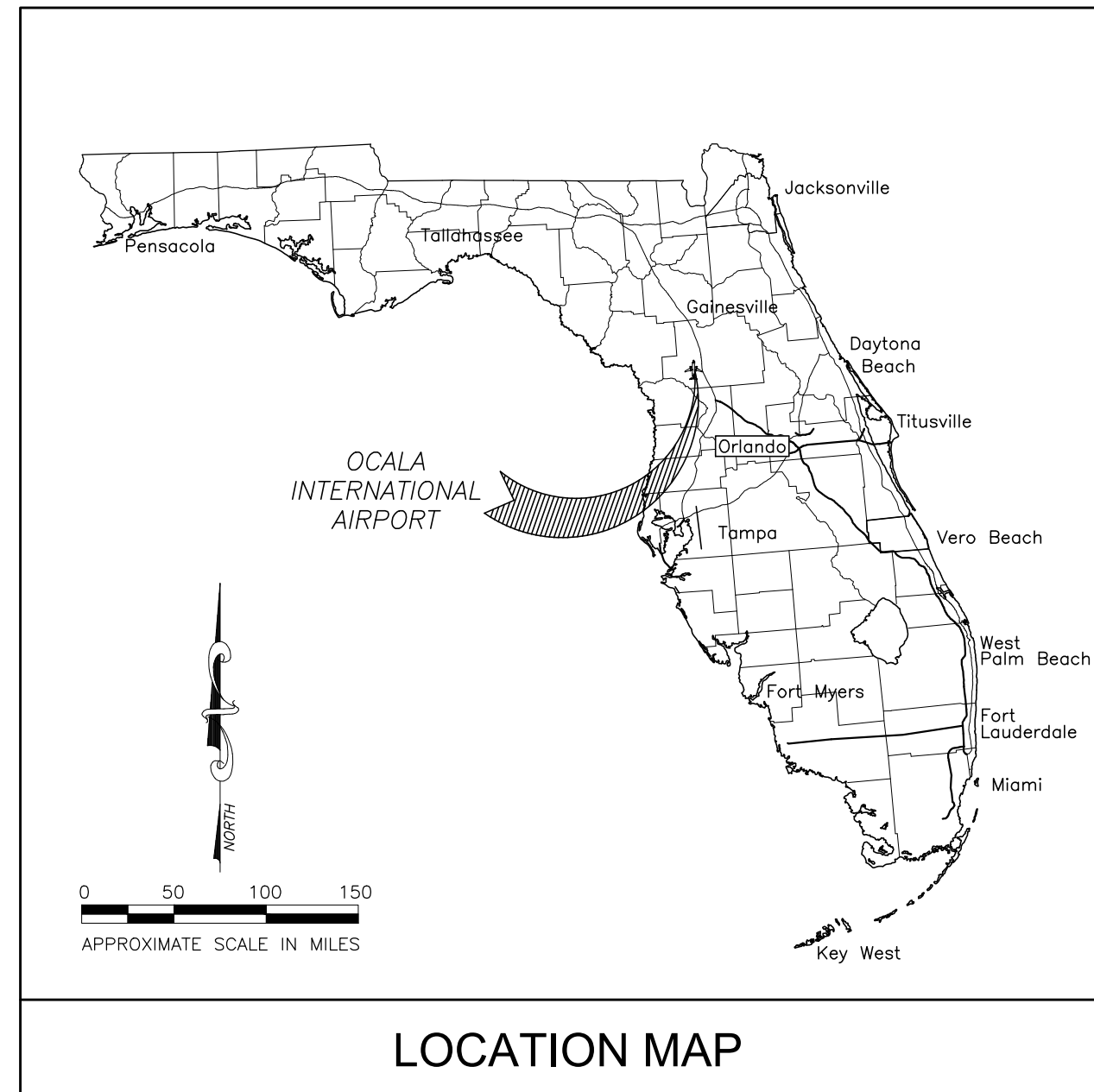


OCALA INTERNATIONAL AIRPORT RUNWAY 36 RSA GRADING

AT



FDOT FINANCIAL PROJECT NO.: 457809-1-94-01
 FAA FUNDING NO.: 3-12-0055-038-2026
 CITY OF OCALA PROJECT NO: AIR/260692



DRAWING INDEX (ELECTRICAL)

SHEET. No.	DESCRIPTION
E0.1	AIRFIELD ELECTRICAL LEGEND AND NOTES
E1.1	MALSR MODIFICATION PLAN
E2.1	MALSR PROFILE PLAN
E3.0	STATION 08 ELECTRICAL DETAILS
E3.1	STATION 08 FOUNDATION AND STRUCTURE DETAILS

THE BELOW NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE ABOVE SHEETS IN ACCORDANCE WITH RULE 61615-23.004, F.A.C.

MARK MORLEY, FL P.E. NO. 59813

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE ABOVE SHEETS IN ACCORDANCE WITH RULE 61615-23.004, F.A.C.
 PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.



BID DOCUMENTS

PREPARED BY:



5550 W. IDLEWILD AVENUE, SUITE 115

TAMPA, FL 33634

(813) 330-2704

CERTIFICATE OF AUTHORIZATION NO.: 30862

DRAWING INDEX

DWG. No.	DESCRIPTION
G0.0	COVER SHEET
G1.0	PROJECT LAYOUT PLAN & HAUL ROUTES
G2.0	GENERAL NOTES
G3.1-G3.2	CONSTRUCTION SAFETY & PHASING PLAN, NOTES AND DETAILS
C1.0	KEY SHEET
C2.0	SURVEY CONTROL PLAN
C3.1	EXISTING CONDITIONS
C4.1	DEMOLITION PLAN
C5.1	GRADING AND DRAINAGE PLAN
C6.1	PLAN & PROFILE
C7.1	SODDING PLAN

DOUG HAMBRECHT, FL P.E. NO. 57145

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE ABOVE SHEETS IN ACCORDANCE WITH RULE 61615-23.004, F.A.C.
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GENERAL NOTES

1. CONSTRUCTION STAKE-OUT SHALL BE PERFORMED BY CONTRACTOR IN ACCORDANCE WITH ARTICLE 50-06 OF THE GENERAL PROVISIONS OF THE CONTRACT DOCUMENTS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ALL MEASUREMENTS THAT MAY BE REQUIRED TO LAY OUT THE CONSTRUCTION. THE COST OF STAKING WILL NOT BE PAID FOR DIRECTLY AND SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS ITEMS OF WORK.
2. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE CLEANUP AND DISPOSAL OF ALL TRASH AND DEBRIS CREATED BY HIS WORK OR PERSONNEL. NO BURNING IS ALLOWED ON SITE. ALL TRASH AND DEBRIS MUST BE DISPOSED OF OFFSITE.
3. THE CONTRACTOR SHALL VISIT SITE TO DETERMINE EXISTING CONDITIONS PRIOR TO SUBMITTING BID. THE CONTRACTOR SHALL REPORT TO THE ENGINEER ANY VARIATIONS FROM THE INFORMATION SHOWN ON THE CONSTRUCTION PLANS.
4. EXISTING EASEMENTS TO OTHER PROPERTIES SHALL BE MAINTAINED AT ALL TIMES.
5. AIRFIELD PAVEMENTS SHALL BE KEPT FREE OF ALL DEBRIS, DIRT, ETC., AT ALL TIMES. ANY SPILLAGE OF EXCAVATION OR OTHER MATERIAL SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR WITH A MOTOR DRIVEN SWEEPER OR VACUUM AS REQUIRED BY THE ENGINEER. A PROGRAM OF REGULAR AIRFIELD PAVEMENT INSPECTION WILL BE PLANNED BY THE CONTRACTOR, AIRPORT OPERATIONS AND THE OWNER'S REPRESENTATIVE. SWEEPERS ARE TO BE EQUIPPED WITH NON-METALLIC BROOMS.
6. ALL NON-PAVED AREAS OUTSIDE THE LIMITS OF CONSTRUCTION WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS, SUCH AS THE CONTRACTOR'S ACCESS ROAD, STAGING AREA, HAUL ROUTES, ETC., SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND GRASSED PER ITEM T-901 UPON COMPLETION OF THE PROJECT.
7. ANY DAMAGES DONE TO AIRPORT PROPERTY OR UTILITIES (SUCH AS RUNWAY, TAXIWAYS, APRONS, FENCING, EXISTING CABLES) WILL BE REPAIRED BY THE CONTRACTOR TO THE APPROVAL OF THE OWNER OF THE FACILITY IN A SATISFACTORY MANNER. THE CONTRACTOR WILL BEAR ALL COSTS FOR REPAIRS USING INDUSTRY STANDARD PRACTICES AND MATERIALS.
8. THE CONTRACTOR SHALL MAINTAIN SECURITY WITHIN THE PROJECT SITE AT ALL TIMES. NO UNAUTHORIZED PERSONNEL SHALL BE ALLOWED ON THE SITE.
9. CONTRACTOR SHALL MAINTAIN ALL AIRFIELD SAFETY DEVICES SUCH AS STAKED LIMIT LINES, FOR THE DURATION OF THE PROJECT AS REQUIRED. DAMAGED STAKES OR FLAGGING SHALL BE REPLACED IMMEDIATELY. CONTRACTOR TO SUBMIT PLAN SHOWING LOCATION OF LIMIT LINES FOR EACH PHASE AND FOR PROJECT DURATION TO THE ENGINEER FOR APPROVAL.
10. THE CONTRACTOR SHALL OBTAIN ALL PERMITS NECESSARY FOR THE COMPLETION OF THIS PROJECT.
11. THE CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH ALL ENVIRONMENTAL RULES AND REGULATIONS OF THE CITY, COUNTY, STATE, ARMY CORPS OF ENGINEERS, AND ANY OTHER JURISDICTIONAL AGENCIES, AND ALL CONDITIONS SET FORTH IN ENVIRONMENTAL PERMITS.
12. AIRCRAFT OPERATING ON THE AIRFIELD MAKE THE CONSTRUCTION AREA A ZONE OF HIGH LEVEL NOISE. THE CONTRACTOR IS ADVISED TO TAKE THE NECESSARY PRECAUTIONS, SUCH AS THE USE OF EAR PLUGS AND EAR MUFFS TO PREVENT EAR INJURY TO ANY PERSONNEL WORKING IN THE AREA.
13. ALL DISPUTES ARISING FROM THE CONTRACTOR SHALL BE DECIDED BY THE ENGINEER, WHOSE DECISION SHALL BE FINAL.
14. BURNING OF DEBRIS WILL NOT BE ALLOWED ON AIRPORT PROPERTY.
15. ALL DEMOLISHED MATERIALS BECOME THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE SHOWN IN THE CONTRACT DOCUMENTS. CONSTRUCTION AND DEMOLITION DEBRIS SHALL BE LEGALLY DISPOSED OF OFF AIRPORT PROPERTY.
16. THE CONTRACTOR SHALL KEEP A WATER TRUCK ONSITE AT ALL TIMES FOR THE PURPOSE OF CONTROLLING DUST AS REQUIRED BY THE CONTRACT DOCUMENTS.
17. ALL CONSTRUCTION WITHIN A RESTRICTED AREA SHALL BE PERFORMED IN SUCH A MANNER THAT, AT THE END OF THE CLOSURE PERIOD, IT WILL LEAVE THE SAFETY AREA WITH NO ABRUPT GRADE CHANGES OR GRADES IN EXCESS OF 5 PERCENT, AND WITH NO TRENCHES WITH DEPTH OR WIDTH GREATER THAN 3 INCHES.

HAUL ROUTE / STAGING

18. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE STORAGE AND SECURITY OF HIS MATERIAL AND EQUIPMENT AND SHALL ERECT STORAGE FACILITIES AND FENCING AS NECESSARY. THE CONTRACTOR'S STORAGE AND STAGING AREA SHALL BE IN THE LOCATION SHOWN ON DRAWING G1.0.
19. THE CONTRACTOR'S STAGING AREA(S) AND HAUL ROUTES SHOWN ON THE PLANS ARE GENERAL AND FOR INFORMATIONAL PURPOSES ONLY. THE ACTUAL SIZE AND LOCATION OF STAGING AREAS AND HAUL ROUTES WILL BE APPROVED BY THE OWNER PRIOR TO CONSTRUCTION. CONTRACTOR SHALL RESTORE THE GROUND OF THE STAGING AREA AT THEIR COST.
20. CONTRACTOR WILL NOT BE ALLOWED TO USE ANY OF THE EXISTING RUNWAYS OR TAXIWAYS AS PART OF THE HAUL ROAD UNLESS SPECIFICALLY AUTHORIZED BY THE OWNER'S REPRESENTATIVE. UNDER NO CIRCUMSTANCE SHALL THE CONTRACTOR CROSS ANY RUNWAY AT ANY TIME WITH CONSTRUCTION VEHICLES OR EQUIPMENT.
21. ALL CONSTRUCTION TRAFFIC SHALL ENTER AND EXIT THE PROJECT AREA THROUGH THE

- PROJECT ACCESS ROUTES APPROVED BY THE ENGINEER ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SECURITY OF ALL GATES HE USES FOR ACCESS ONTO AIRPORT PROPERTY. THE CONTRACTOR SHALL POST A GUARD AT ANY ACCESS GATE THAT IS NOT LOCKED.
22. THE OWNER'S REPRESENTATIVE SHALL DESIGNATE AREAS TO BE USED BY THE CONTRACTOR FOR THE PARKING OF CONSTRUCTION EQUIPMENT AND VEHICLES WHEN NOT ENGAGED IN THE CONSTRUCTION DURING NON-WORKING DAYS AND NIGHTS AS WELL AS AREAS FOR CONTRACTOR'S EMPLOYEES AUTO PARKING.
 23. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PROVIDING ALL PERMANENT AND TEMPORARY UTILITY CONNECTIONS TO THE STAGING AREA.

MOVEMENT ON AIRFIELD

24. AREAS OUTSIDE THE PROJECT LIMITS ARE DESIGNATED AS RESTRICTED AREAS. THE CONTRACTOR'S FORCES ARE PROHIBITED FROM ENTERING RESTRICTED AREAS AT ANY TIME, UNLESS SPECIFICALLY AUTHORIZED BY THE OWNER'S REPRESENTATIVE OR AIRPORT OPERATIONS.
25. THE CONTRACTOR SHALL CONDUCT HIS CONSTRUCTION OPERATIONS AS SHOWN ON THE CONSTRUCTION SAFETY & PHASING PLAN, AND HIS APPROVED PHASING PLAN. THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH THE OWNER'S REPRESENTATIVE TO MINIMIZE INTERFERENCE TO AIRCRAFT OPERATIONS DURING CONSTRUCTION.
26. NO CONTRACTOR VEHICLES, EQUIPMENT, OR PERSONNEL SHALL CROSS OR BE WITHIN ANY RUNWAY SAFETY AREA OR WITHIN ANY TAXIWAY OBJECT FREE AREA AS DELINEATED IN THE PLANS. THE CONTRACTOR SHALL NOT PARK OR LEAVE UNATTENDED ANY EQUIPMENT WITHIN THE RUNWAY OBJECT FREE AREA (ROFA) OR THE RUNWAY PROTECTION ZONE (RPZ). EQUIPMENT NOT MANNED AND BEING USED SHALL NOT BE ALLOWED WITHIN THE ROFA OR RPZ.
27. ALL VEHICLES USED ON THE AIRFIELD SHALL MEET FAA REQUIREMENTS FOR MARKINGS AND LIGHTING. (SEE AC 150/5210-5D).
28. ALL ACTIVE AIRPORT OPERATIONAL AREAS WHICH ARE ADJACENT TO A CONSTRUCTION WORK AREA SHALL BE SEPARATED BY TYPE II LOW PROFILE BARRICADES OR OTHER BARRIER APPROVED BY THE ENGINEER. NO CONSTRUCTION TRAFFIC WILL CROSS ACTIVE AIRPORT OPERATIONAL AREAS. THE CONTRACTOR SHALL PROVIDE AND INSTALL THE BARRIERS AS SHOWN ON THE DRAWINGS AND WHERE DIRECTED BY THE ENGINEER. SEE SPECIFICATION 01030 AND 01530 FOR ADDITIONAL INFORMATION.
29. THE CONTRACTOR SHALL WORK WITH THE ENGINEER IN SCHEDULING ALL TAXIWAY OPENINGS/CLOSINGS TO BE CONSISTENT WITH AIRFIELD OPERATIONS. WHEN THE CONTRACTOR'S OPERATIONS CROSS ACTIVE TAXIWAYS, HE SHALL BE REQUIRED TO PROVIDE A FLAGMAN ON EACH SIDE OF THE ACTIVE CROSSING TO DIRECT VEHICULAR TRAFFIC AND PROTECT AIRCRAFT TRAFFIC. THE CONTRACTOR SHALL ALSO HAVE SUITABLE EQUIPMENT APPROVED BY THE ENGINEER AT EACH CROSSING TO KEEP THE CROSSING CLEAN OF ALL DIRT AND DEBRIS AND SAFE FOR AIRCRAFT TRAFFIC.

BURIED UTILITIES

30. PRIOR TO DIGGING ANY TRENCHES, THE CONTRACTOR SHALL NOTIFY AIRPORT FOR ALL UTILITIES (ELECTRIC, GAS, TELEPHONE, WATER, FIBER, AND ETC.) AND OBTAIN LOCATIONS OF UNDERGROUND UTILITIES.
31. CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND IDENTIFICATION OF ALL EXISTING UTILITIES AND UNDERGROUND PIPELINES IN CONSTRUCTION AREA. ANY DAMAGES TO EXISTING UTILITIES OR UNDERGROUND PIPELINES ON OR OFF AIRPORT PROPERTY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ALL REPAIR WORK SHALL MEET THE APPROVAL OF THE OWNER OF THE DAMAGED UTILITY. NO REIMBURSEMENT WILL BE ALLOWED FOR UTILITY/PIPE REPAIR OR REPLACEMENT.
32. THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES, AIRFIELD LIGHTING AND NAVAIDS NOT CALLED OUT TO BE REMOVED OR ABANDONED. ANY DAMAGES DONE TO AIRPORT PROPERTY OR UTILITIES (INCLUDING, BUT NOT LIMITED TO RUNWAYS, TAXIWAYS, APRONS, FENCING, EXISTING CABLES, LIGHTING, SIGNS, NAVAIDS) WILL BE REPAIRED BY THE CONTRACTOR TO THE APPROVAL OF THE OWNER OF THE FACILITY IN A SATISFACTORY MANNER. THE CONTRACTOR WILL BEAR ALL COSTS FOR REPAIRS.
33. ANY UNPLANNED, UNAPPROVED, OR ACCIDENTAL SHUTDOWN OR INTERRUPTION OF SERVICE TO ANY LIGHTING CIRCUIT OR NAVIGATIONAL AID REQUIRES IMMEDIATE NOTIFICATION OF AIRPORT OPERATIONS AND THE OWNER'S REPRESENTATIVE BY THE CONTRACTOR. THE COST OF MATERIALS AND LABOR REQUIRED TO REPAIR THE LIGHTING CIRCUIT SHALL BE BORNE BY THE CONTRACTOR.

LIQUIDATED DAMAGES

36. LIQUIDATED DAMAGES AS SHOWN ON THE CITY'S BID DOCUMENTS SHALL BE ASSESSED FOR:
 - A. EACH DAY BEYOND THE SPECIFIED CONTRACT TIME FOR EACH PHASE THAT IS NOT SUBSTANTIALLY COMPLETE.
- BADGING**
37. A MINIMUM OF ONE EMPLOYEE MUST BE BADGED TO SUPERVISE CREWS AT ALL TIMES. NO COST FOR AIRPORT BADGE.



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TAMPA, FLORIDA 33634 (813) 330-2701
CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:

RUNWAY 36 RSA GRADING

Designer: JM	Checked By: MA
Technician: JM	ICE Project Name: 26-031

Engineer of Record:

NOTES:

REVISIONS

NO.	DESCRIPTION	DATE	BY

Drawing Name:

GENERAL NOTES

FAA A.I.P. Project No.:
341-334-000-000-09-33173

FDOT Project No.:
341-334-000-000-09-33173

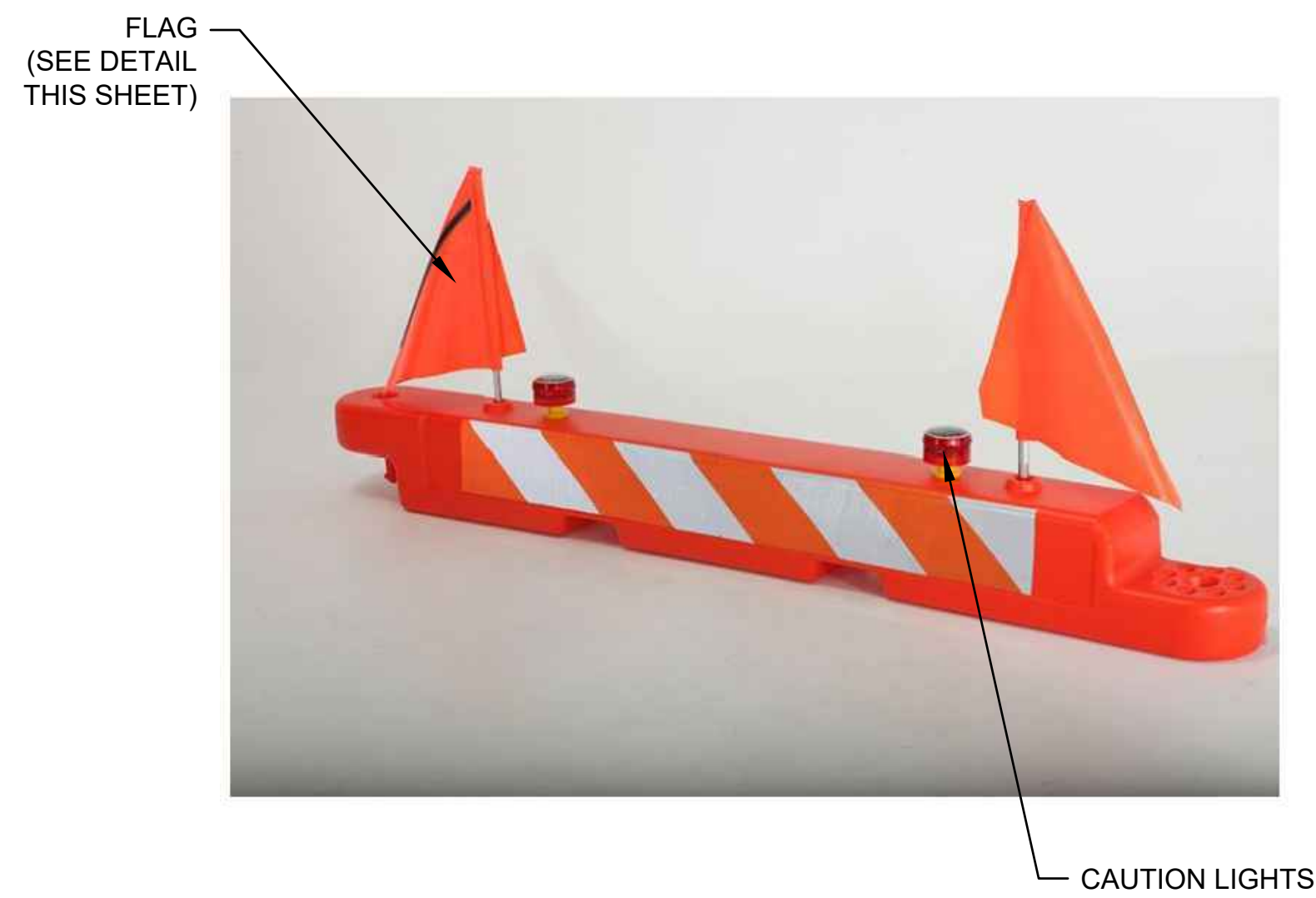
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90% DESIGN DOCUMENTS - NOT FOR CONSTRUCTION

NOTES

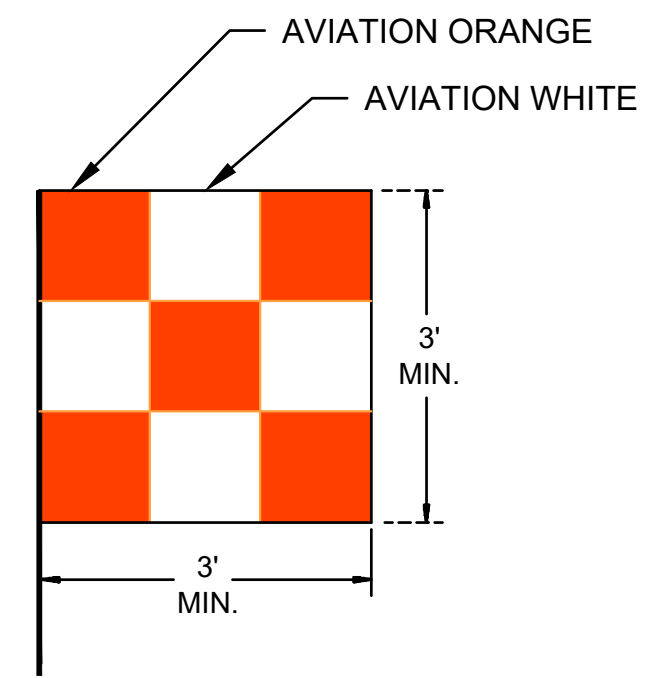
1. THE CONTRACTOR SHALL OBTAIN, HAVE KNOWLEDGE OF, AND INCORPORATE THE FOLLOWING SAFETY PROVISIONS INTO THE CONSTRUCTION PROJECT:
 - 1.1. OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION - AC 150/5370-2, LATEST EDITION.
 - 1.2. AIRPORT SAFETY SELF-INSPECTION - AC 150/5200-18, LATEST EDITION.
 - 1.3. PAINTING, MARKING AND LIGHTING OF VEHICLES USED ON AIRPORTS - AC 150/5210-5, LATEST EDITION.
 - 1.4. CODE OF FEDERAL REGULATIONS (CFR) PART 139
2. NOTICE TO AIRMEN (NOTAM) - THE AIRPORT WILL ISSUE ALL NOTAMS TO THE FAA FLIGHT SERVICE STATION RELATED TO THIS PROJECT IN ACCORDANCE WITH ESTABLISHED CRITERIA.
3. FOR OTHER CONDITIONS RELATED TO SAFETY, SEE SPECIFICATIONS.
4. PRIOR TO THE CLOSURE OF ANY AIRFIELD PAVEMENT, THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 3 DAYS NOTICE SO A NOTAM CAN BE ISSUED.
5. AIRCRAFT TRAFFIC SHALL HAVE THE RIGHT-OF-WAY AT ALL TIMES.
6. THE CONTRACTOR WILL NOT BE ALLOWED TO USE ANY OF THE EXISTING RUNWAYS, TAXIWAYS, OR VEHICLE PARKING AREAS AS PART OF THE HAUL ROAD UNLESS SPECIFICALLY AUTHORIZED BY THE OWNER.
7. THE CONTRACTOR SHALL CONDUCT THEIR CONSTRUCTION OPERATIONS AS SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH THE OWNER TO MINIMIZE INTERFERENCE TO AIRCRAFT OPERATIONS DURING CONSTRUCTION.
8. NO CONTRACTOR VEHICLES, EQUIPMENT, OR PERSONNEL SHALL CROSS OR BE WITHIN ANY RUNWAY, TAXIWAY, OR TAXILANE OBJECT FREE AREA UNLESS SPECIFICALLY AUTHORIZED BY THE OWNER.
9. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER IN SCHEDULING ALL AIRFIELD PAVEMENT OPENINGS/CLOSINGS TO BE CONSISTENT WITH AIRFIELD OPERATIONS. WHEN THE CONTRACTOR'S OPERATIONS CROSS ACTIVE TAXIWAYS OR RUNWAYS HE SHALL BE REQUIRED TO PROVIDE A FLAGMAN ON EACH SIDE OF ACTIVE CROSSING TO DIRECT VEHICULAR TRAFFIC AND PROTECT AIRCRAFT TRAFFIC. THE CONTRACTOR SHALL ALSO HAVE SUITABLE EQUIPMENT APPROVED BY THE ENGINEER AT EACH CROSSING TO KEEP THE CROSSING CLEAN OF ALL DIRT AND DEBRIS AND SAFE FOR AIRCRAFT TRAFFIC.
10. ALL AIRFIELD PAVEMENTS SHALL BE KEPT FREE OF DIRT AND ALL OTHER DEBRIS AT ALL TIMES. ANY SPILLAGE OF EXCAVATION OF OTHER MATERIAL SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR USING A VACUUM OR MOTOR-DRIVEN SWEEPER. A PROGRAM OF REGULAR INSPECTION OF AIRFIELD PAVEMENTS SHALL BE IMPLEMENTED AS PART OF THE CONTRACTOR'S SAFETY PLAN COMPLIANCE DOCUMENT.
11. ANY UNPLANNED, UNAPPROVED, OR ACCIDENTAL SHUTDOWN OR INTERRUPTION OF SERVICE TO ANY LIGHTING CIRCUIT OR NAVIGATIONAL AID REQUIRES IMMEDIATE NOTIFICATION OF THE AIRPORT MANAGER AND ENGINEER BY THE CONTRACTOR. THE COST OF MATERIALS AND LABOR REQUIRED TO REPAIR THE LIGHTING CIRCUIT SHALL BE BORNE BY THE CONTRACTOR.
12. THE CONTRACTOR SHALL PROTECT ALL NAVIGATIONAL AIDS (NAVAIDS) DURING CONSTRUCTION. CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY DAMAGE TO EXISTING NAVAIDS. ANY DAMAGE TO NAVAIDS MAY REQUIRE SUBMITTALS FOR REPAIR OF THE DAMAGED FACILITY TO BE APPROVED BY THE ENGINEER. THE ENGINEER SHALL BE NOTIFIED OF ANY DAMAGE AND IMMEDIATE REPAIR AT THE CONTRACTOR'S EXPENSE WILL BE REQUIRED.

13. CONTRACTOR SHALL MAINTAIN ALL AIRFIELD SAFETY DEVICES SUCH AS STAKED LIMIT LINES, FOR THE DURATION OF THE PROJECT AS REQUIRED. DAMAGED STAKES OR FLAGGING SHALL BE REPLACED IMMEDIATELY. CONTRACTOR TO SUBMIT PLAN SHOWING LOCATION OF LIMIT LINES FOR EACH PHASE AND FOR PROJECT DURATION TO THE ENGINEER FOR APPROVAL.
14. ALL ACTIVE AIRPORT OPERATIONAL AREAS WHICH ARE ADJACENT TO A CONSTRUCTION WORK AREA SHALL BE SEPARATED BY LOW PROFILE BARRICADES WITH RED FLASHING LIGHTS. THE CONTRACTOR SHALL PROVIDE, MAINTAIN, AND INSTALL THE BARRICADES.
15. BARRICADES USED TO CLOSE NON-AIRFIELD AREAS SUCH AS ROADS OR PARKING LOTS SHALL CONFORM TO FDOT INDEX 600. ACCEPTABLE OPTIONS INCLUDE PLASTIC DRUMS, TYPE I BARRICADE, TYPE II BARRICADE, OR TYPE III BARRICADE.
16. THE CONTRACTOR SHALL PROVIDE TEMPORARY R11-2, 48 x 30 "ROAD CLOSED" SIGNS TO BE USED IN CONJUNCTION WITH TRAFFIC BARRELS AT ALL LOCATIONS TO BE CLOSED TO VEHICULAR TRAFFIC.
17. CONTRACTOR MAY AT HIS OPTION PROVIDE TEMPORARY CHAIN LINK FENCE AROUND THE CONTRACTOR'S STAGING AREA. THE CITY ASSUMES NO RESPONSIBILITY FOR THE SECURITY OF EQUIPMENT, MATERIAL, OR ANY OTHER ELEMENTS RELATED TO THE CONTRACTOR'S OPERATIONS.
18. THE CONTRACTOR SHALL PROMINENTLY DISPLAY AVIATION SAFETY FLAGS ON ALL CONSTRUCTION EQUIPMENT AT THE HIGHEST POINT ON EACH PIECE OF EQUIPMENT. ALL VEHICLES MUST BE MARKED WITH CONTRACTOR OR SUBCONTRACTOR COMPANY NAME.
19. FOR OTHER CONDITIONS RELATING TO SAFETY, SEE CONSTRUCTION SAFETY AND PHASING DOCUMENT (CSPP).
20. GATES MUST REMAIN CLOSED AT ALL TIMES UNLESS GUARDED BY A COMPETENT EMPLOYEE OF THE CONTRACTOR.



LOW PROFILE BARRICADE DETAIL
N.T.S.

- NOTES:
- 1) ALL BARRICADES SHALL BE LINKED END TO END IN ALL LOCATIONS.
 - 2) FRANGIBLE, LOW PROFILE BARRICADES SHALL BE USED WHERE WORK IS ADJACENT TO ACTIVE AIRCRAFT OR INSIDE OF AN ACTIVE TAXIWAY'S SAFETY AREA. CAUTION LIGHTS TO BE RED IN COLOR AND FLASHING DURING HOURS OF DARKNESS.
 - 3) SECURE EACH BARRICADES WITH TWO SAND BAGS OR USE WATER FILLED.

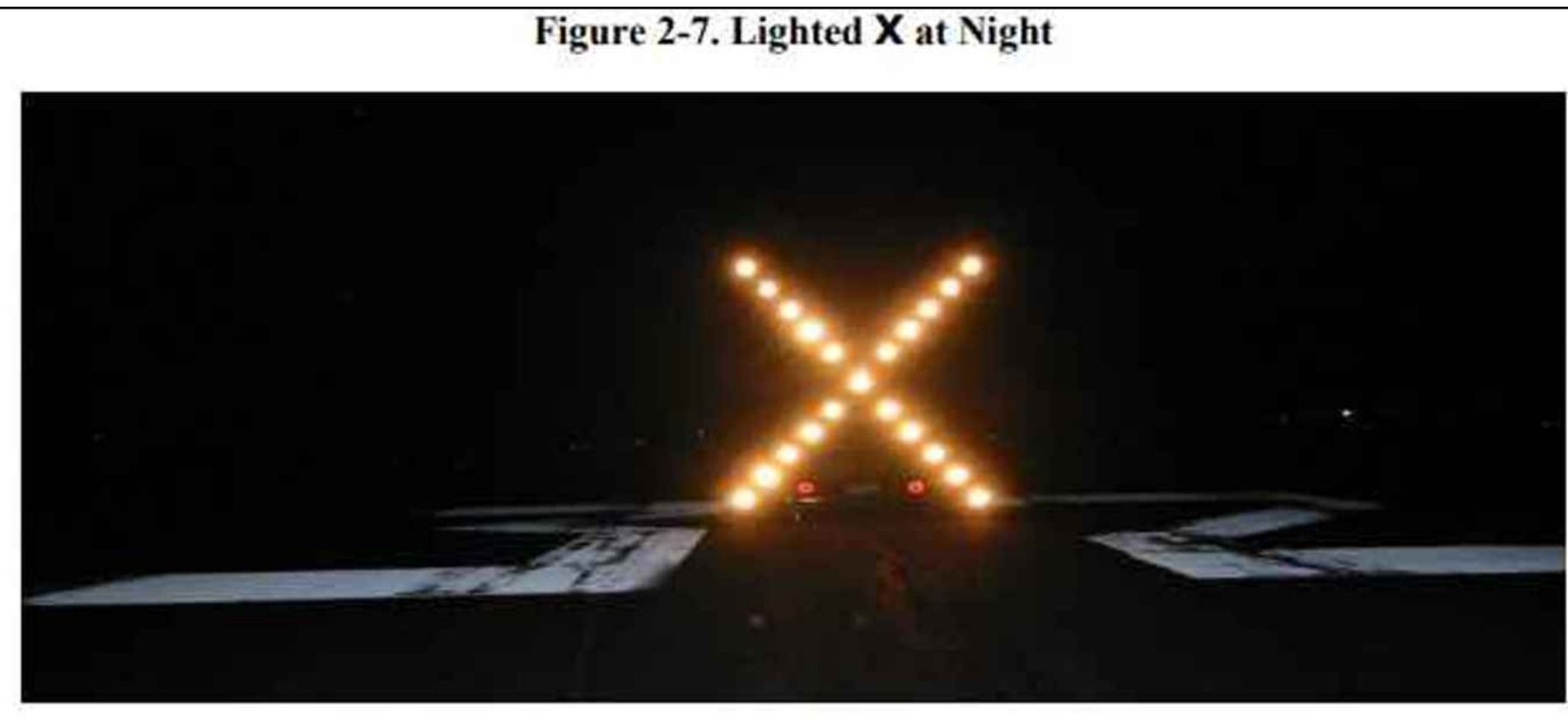


CONSTRUCTION SAFETY FLAG

NOTE:
SAFETY FLAG SHALL BE PROMINENTLY DISPLAYED ON ALL CONSTRUCTION EQUIPMENT. AN AMBER FLASHING LIGHT IS REQUIRED FOR NIGHT WORK AND MAY BE SUBSTITUTED FOR THE FLAG DURING THE DAY.



TRAFFIC CONE
N.T.S.



LIGHTED "X" RUNWAY CLOSURE
N.T.S.



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TAMPA, FLORIDA 33634 (813) 330-2701
CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:
RUNWAY 36 RSA GRADING

Designer: JM	Checked By: MA
Technician: JM	ICE Project Name: 26-031

Engineer of Record:

NOTES:

REVISIONS			
NO.	DESCRIPTION	DATE	BY

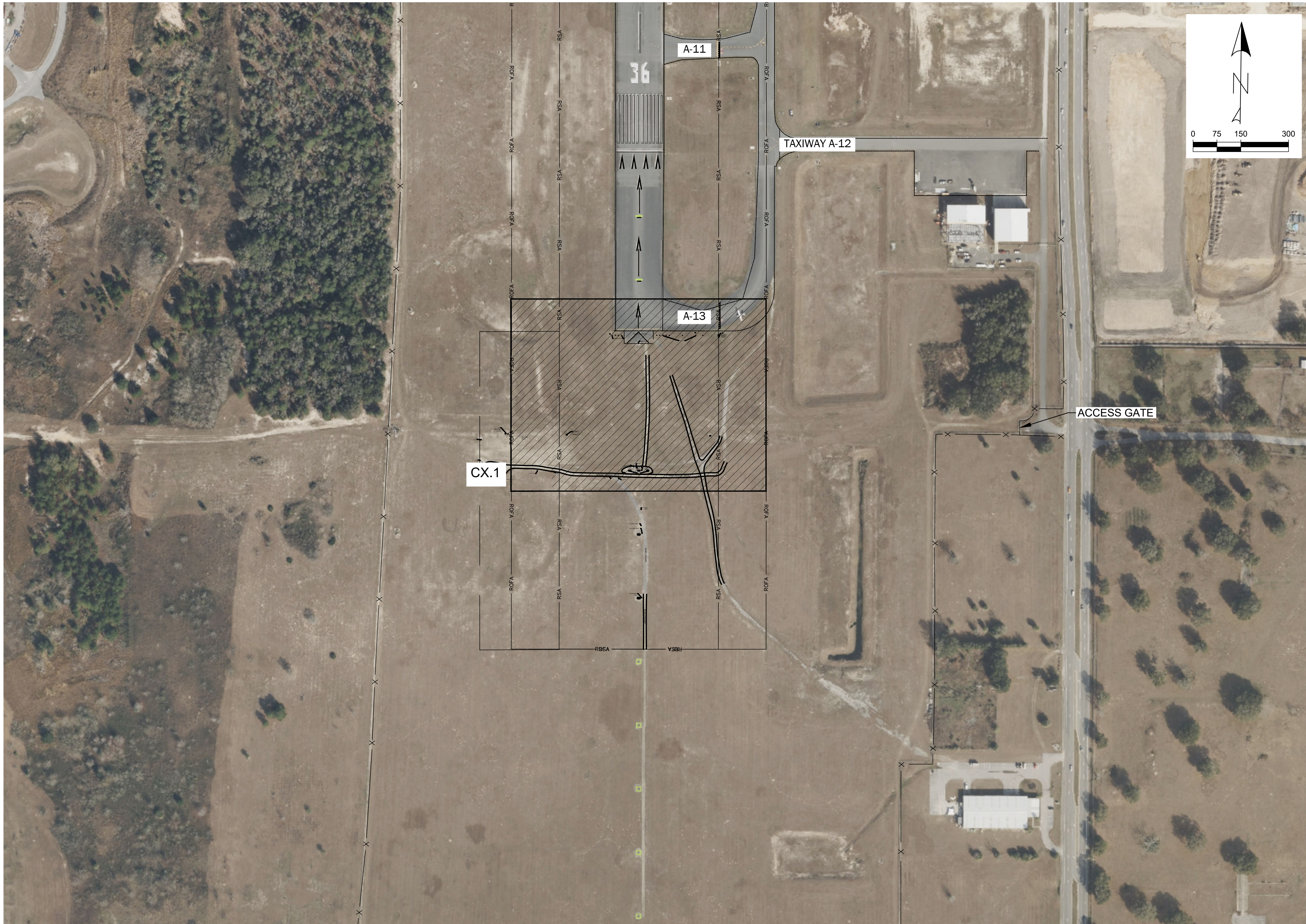
Drawing Name:
CSPP NOTES & DETAILS

FAA A.I.P. Project No.:
341-334-000-000-09-33173

FDOT Project No.:
341-334-000-000-09-33173

Date: APRIL 2026	Sheet Number: G3.2
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5550 WEST IDLEWILD AVE. SUITE 115
TAMPA, FLORIDA 33634 (813) 330-2701
CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:

RUNWAY 36 RSA GRADING

Designer:
JM

Checked By:
MA

Technician:
JM

ICE Project Name:
26-031

Engineer of Record:

NOTES:

REVISIONS

NO.	DESCRIPTION	DATE	BY

Drawing Name:

KEY MAP

FAA A.I.P. Project No.:

341-334-000-000-09-33173

FDOT Project No.:

341-334-000-000-09-33173

Date:

APRIL 2026

Sheet Number:

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90% DESIGN DOCUMENTS - NOT FOR CONSTRUCTION

GENERAL SURVEY REQUIREMENTS:

1. THE NEED FOR PRECISE SURVEY CONTROLS IS OF THE UTMOST IMPORTANCE ON THE PROJECT TO VALIDATE QUANTITIES AND MAINTAIN THE OVERALL PROJECT BUDGET. THE CONTRACTOR, IN ADDITION TO VERIFYING THE DESIGN SURVEY CONTROLS, SHALL ALSO VERIFY THE TOPOGRAPHIC SURVEY. THE DESIGN SURVEY SHALL BE MADE AVAILABLE TO THE CONTRACTOR WITH FULL EXPECTATION OF COMPLIANCE. ANY DISCREPANCIES WITH THE DESIGN SURVEY SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER FOR RESOLUTION PRIOR TO PROCEEDING WITH ANY FIELD WORK. NO CONSTRUCTION WORK MAY BEGIN UNTIL THE SURVEY VERIFICATION IS COMPLETED.
2. CONTRACTOR'S SURVEY: THE CONTRACTOR WILL BE GIVEN THE DESIGN SURVEY FOR HIS/HER REVIEW PRIOR TO LAYOUT. UPON VERIFICATION OF THE DESIGN SURVEY THE CONTRACTOR SHALL SET UP A LONGITUDINAL AND TRANSVERSE GRID WHICH MATCHES THE DESIGN SURVEY GRID ON THE PAVED SURFACES (ALONG THE STATIONING) EXTENDING OUTWARD TO THE EDGE OF THE WORK. THE HORIZONTAL (X, Y) AND THE VERTICAL (Z) COORDINATES OF EACH POINT ON THE GRID SHALL BE PLOTTED AND SUBMITTED TO THE ENGINEER FOR REVIEW AT LEAST 48 HOURS PRIOR TO ANY CONSTRUCTION ACTIVITIES. SURVEY SHALL BE PROVIDED IN ELECTRONIC FORMAT (AUTOCAD CIVIL 3D).

SURVEY CONTROL NOTES:

1. THE CONTRACTOR SHALL VERIFY ALL CONTROL POINTS TO BE USED FOR CONSTRUCTION PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITY. SHOULD ANY DISCREPANCIES BE FOUND, THE CONTRACTOR SHALL IMMEDIATELY ALERT THE ENGINEER AND SHALL NOT PROCEED WITH CONSTRUCTION UNTIL THE DISCREPANCIES ARE RESOLVED.
2. THE CONTRACTOR SHALL PROTECT ALL CONSTRUCTION CONTROL POINTS DURING THE ENTIRE DURATION OF CONSTRUCTION. SHOULD SOME CONTROL POINTS BE DISLODGED, THE CONTRACTOR SHALL RE-SET THEM.

SURVEY CONTROL POINTS:

BM-1
N: 1753964.6440'
E: 584574.6840'
ELEV.: 65.23'

BM-2
N: 1753935.0300'
E: 585419.6290'
ELEV.: 72.37'

BM-3
N: 1752903.6160'
E: 585388.6730'
ELEV.: 65.72'

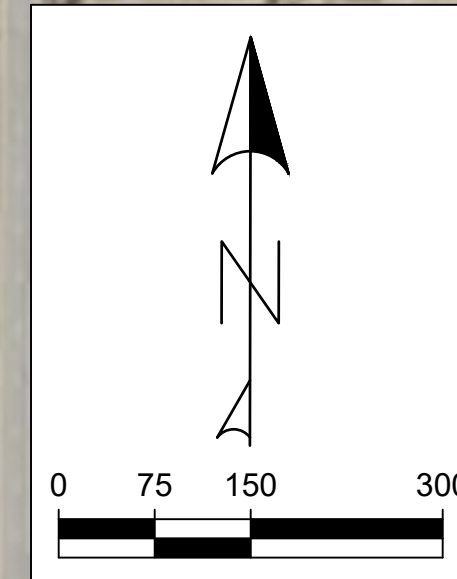
BM-4
N: 1752878.8670'
E: 584570.6040'
ELEV.: 75.72'

HORIZONTAL PROJECTION
NAD83 FLORIDA STATE PLANES,
WEST ZONE, US FOOT

VERTICAL DATUM
NAVD88

SURVEY NOTES:

1. DATE OF FIELD SURVEY: FEBRUARY 12, 2026
2. SUBJECT TO RIGHTS OF WAY, RESTRICTIONS, EASEMENTS AND RESERVATIONS OF RECORD.
3. UNLESS OTHERWISE SHOWN, UNDERGROUND IMPROVEMENTS NOT LOCATED.
4. PUBLIC RECORDS NOT SEARCHED BY R.M. BARRINEAU & ASSOCIATES, INC.
5. BEARINGS AND STATE PLANE COORDINATES DEPICTED HEREON ARE GRID, WEST FLORIDA ZONE, NAD-83 (CORS96) EPOCH:2002.0000, BASED ON TRIMBLE VIRTUAL REFERENCE STATION NETWORK AND REFERENCED TO CITY OF OCALA ENGINEERING DEPARTMENT CONTROL POINT 0011.
6. VERTICAL DATUM BASED ON CITY OF OCALA ENGINEERING DEPARTMENT CONTROL POINT 0011 WITH AN ELEVATION OF 85.20', NAVD-88.
7. ORIENTATION FOR THE IMPROVEMENTS SHOWN HEREON SHOULD NOT BE USED TO RECONSTRUCT BOUNDARY LINES.
8. ADDITIONS OR DELETIONS TO SURVEY MAPS BY OTHER THAN THE SIGNING PARTY OR PARTIES IS PROHIBITED WITHOUT WRITTEN CONSENT OF THE SIGNING PARTY OR PARTIES.
9. THIS SURVEY DEPICTS THE PROPERTY AS IT EXISTED ON THE SURVEY DATE, NOT NECESSARILY THE SIGNATURE DATE.
10. THIS SURVEY HAS BEEN PREPARED FOR THE EXCLUSIVE BENEFIT OF THE PARTY(IES) NAMED HEREON, AND SHALL NOT BE DUPLICATED OR RELIED UPON BY ANY OTHER INDIVIDUAL OR ENTITY WITHOUT AUTHORIZATION FROM R.M. BARRINEAU & ASSOCIATES, INC.



5550 WEST IDLEWILD AVE. SUITE 115
TAMPA, FLORIDA 33634 (813) 330-2701
CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:

RUNWAY 36 RSA GRADING

Designer: JM

Checked By: MA

Technician: JM

ICE Project Name: 26-031

Engineer of Record:

NOTES:

REVISIONS

NO.	DESCRIPTION	DATE	BY

Drawing Name:

SURVEY CONTROL PLAN

FAA A.I.P. Project No.:

341-334-000-000-09-33173

FDOT Project No.:

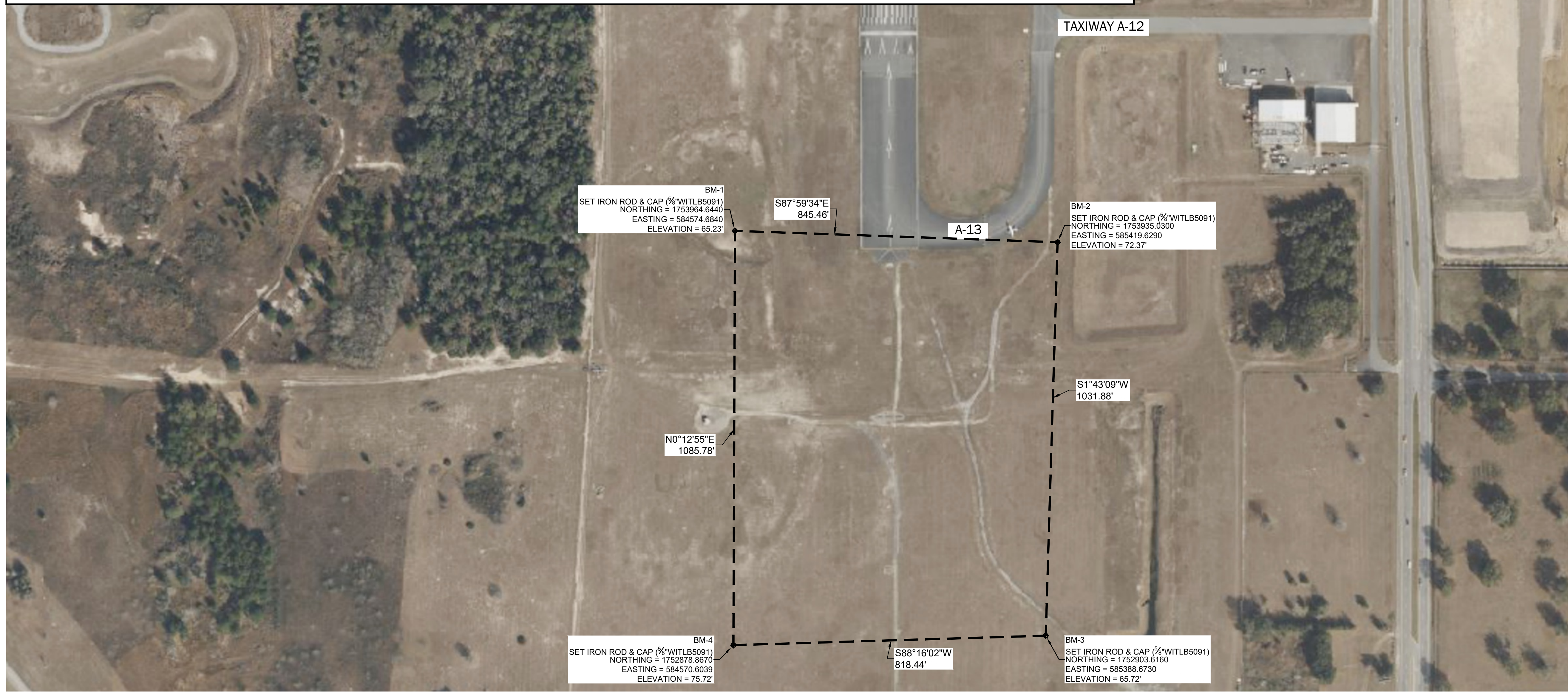
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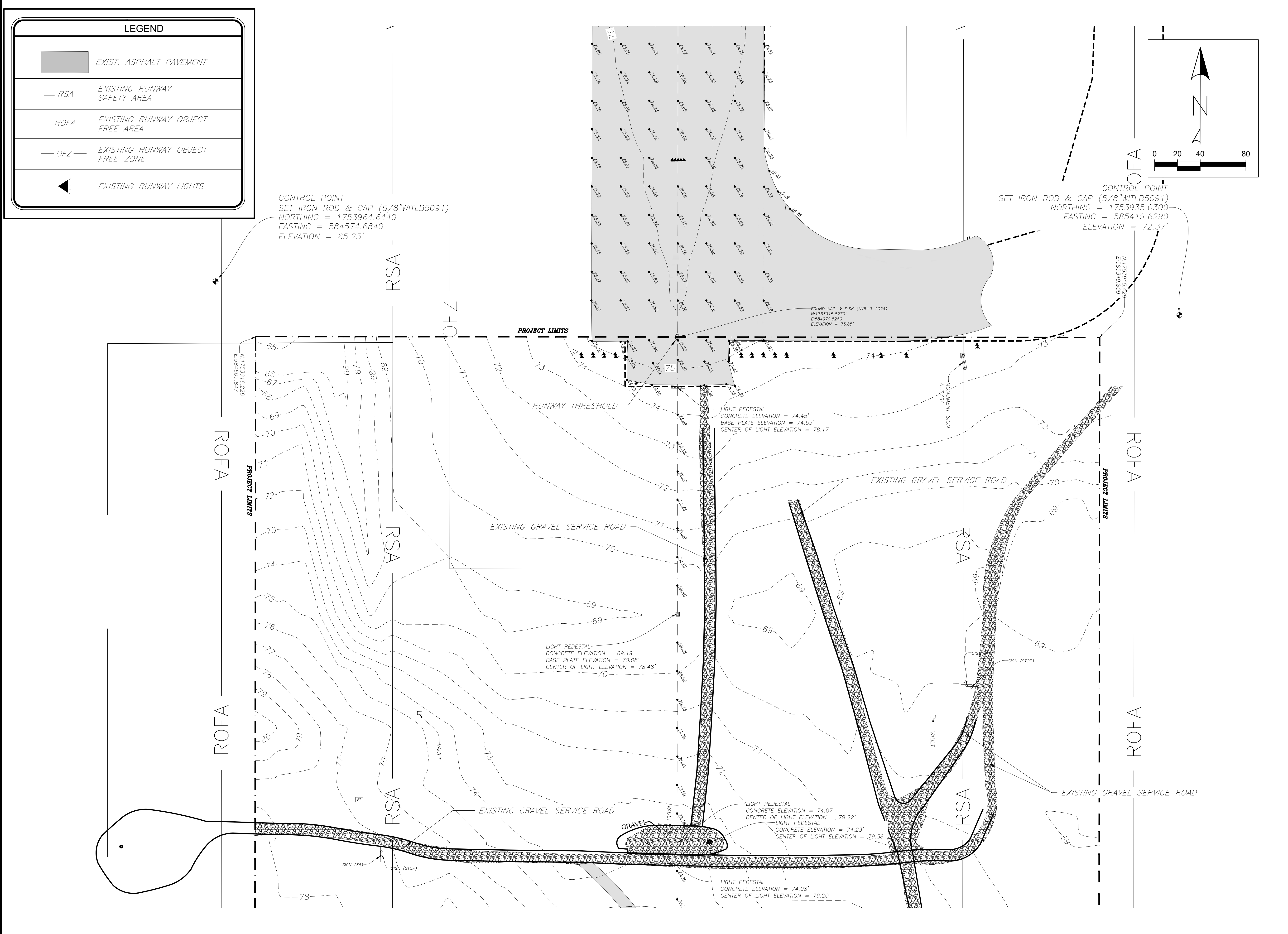
APRIL 2026

Sheet Number:

C2.0

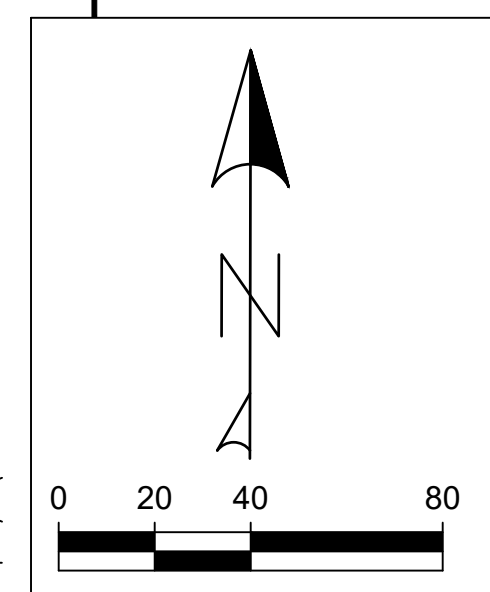


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LEGEND

- EXIST. ASPHALT PAVEMENT
- RSA — EXISTING RUNWAY SAFETY AREA
- ROFA — EXISTING RUNWAY OBJECT FREE AREA
- OFZ — EXISTING RUNWAY OBJECT FREE ZONE
- EXISTING RUNWAY LIGHTS



5550 WEST IDLEWILD AVE. SUITE 115
TAMPA, FLORIDA 33634 (813) 330-2701
CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:
RUNWAY 36 RSA GRADING

Designer: JM Checked By: MA
Technician: JM ICE Project Name: 26-031
Engineer of Record:

NOTES:

REVISIONS

NO.	DESCRIPTION	DATE	BY

Drawing Name:
EXIST. CONDITION PLAN

FAA A.I.P. Project No.:
341-334-000-000-09-33173

FDOT Project No.:
341-334-000-000-09-33173

Date: **APRIL 2026** Sheet Number: **C3.1**

90% DESIGN DOCUMENTS - NOT FOR CONSTRUCTION



5550 WEST IDLEWILD AVE. SUITE 115
TAMPA, FLORIDA 33634 (813) 330-2701
CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:
RUNWAY 36 RSA GRADING

Designer: JM
Checked By: MA
Technician: JM
ICE Project Name: 26-031

Engineer of Record:

NOTES:

REVISIONS

NO.	DESCRIPTION	DATE	BY

Drawing Name:
GRADING & DRAINAGE PLAN

FAA A.I.P. Project No.:
341-334-000-000-09-33173

FDOT Project No.:
341-334-000-000-09-33173

Date: APRIL 2026
Sheet Number: C5.1

LEGEND

- EXIST. ASPHALT PAVEMENT
- RSA — EXISTING RUNWAY SAFETY AREA
- ROFA — EXISTING RUNWAY OBJECT FREE AREA
- OFZ — EXISTING RUNWAY OBJECT FREE ZONE
- EXISTING RUNWAY LIGHTS

EARTHWORK SUMMARY

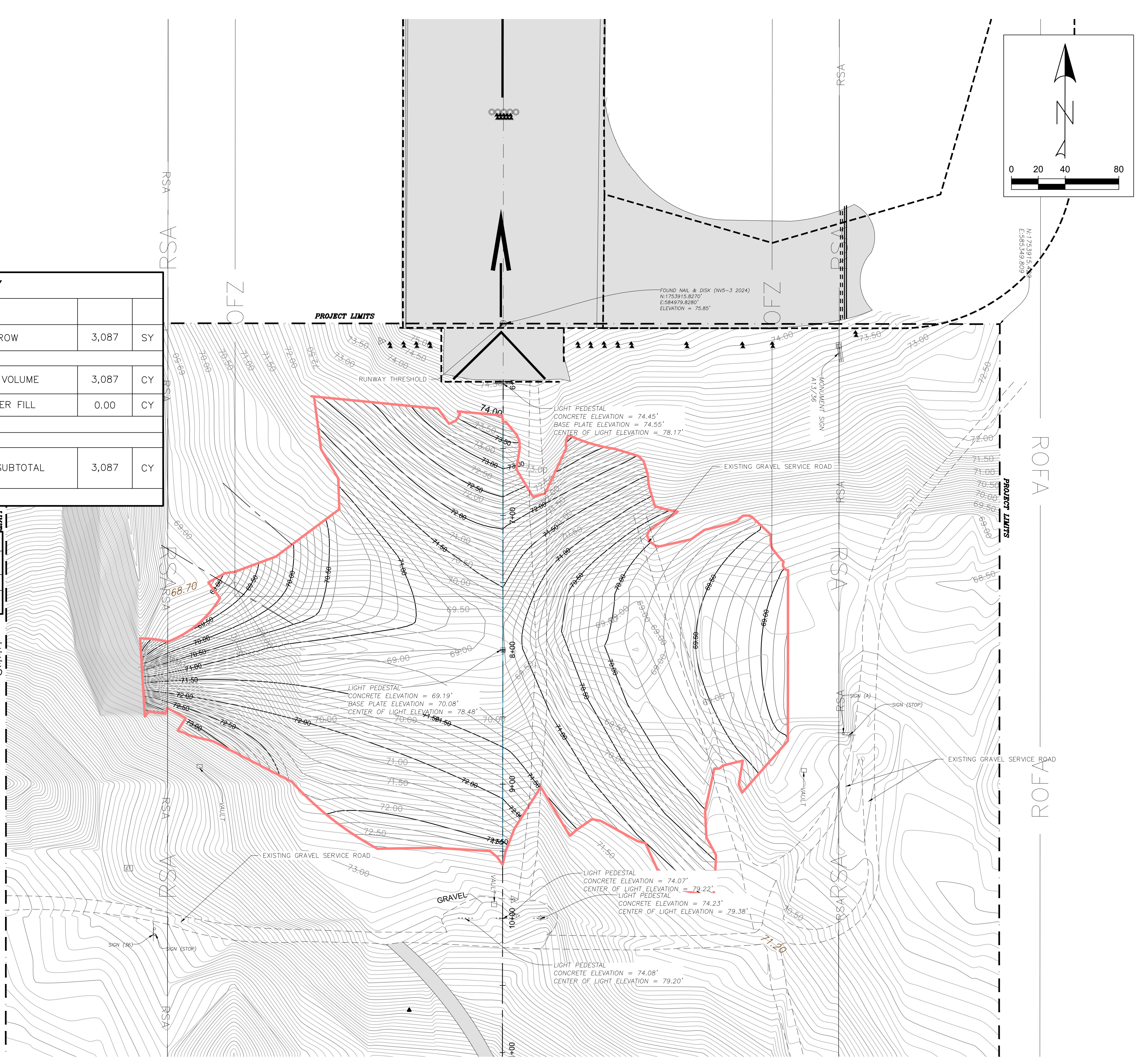
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(a) EXCAVATION	0.00	CY	(d) NET VOLUME	3,087	CY
(b) STRIPPING	2,088	CY	(e) OTHER FILL	0.00	CY
(c) UNSUITABLE	0.00	CY			
(f) EXCAVATION SUBTOTAL	2,088	CY	(g) FILL SUBTOTAL	3,087	CY
(g-a) BORROW	3,087	CY			

NOTE: TOTAL TOPSOIL AVAILABLE ON SITE: 2,088 CY

BORROW ADJUSTMENT

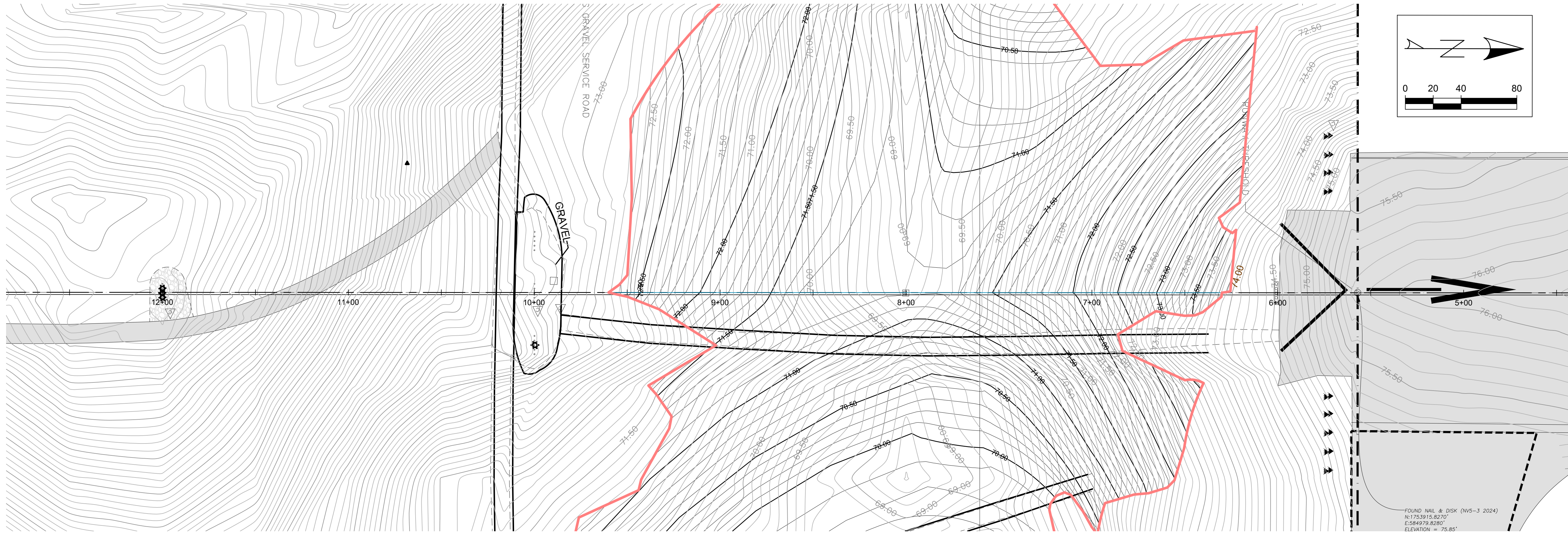
COMPACTED BORROW REQUIRED	3,087	CY
SHRINKAGE FACTOR	15%	
LOOSE BORROW REQUIRED	3,632	CY

NOTE:
EARTHWORK QUANTITIES ARE BASED ON FINAL SURFACE VOLUME CALCULATIONS AND INCLUDE 6-INCH STRIPPING OF TOPSOIL OVER THE ENTIRE PROJECT AREA. STRIPPED TOPSOIL SHALL BE STOCKPILED ON SITE FOR REUSE. A 15 PERCENT SHRINKAGE FACTOR HAS BEEN APPLIED TO BORROW FOR ESTIMATING LOOSE BORROW QUANTITIES.



File Path: C:\Users\marin.adhaka\Bowl\CETPA\AVIATION\Ocala\26-031 RSA Grading\CADD\dwg\001\01_C5.1-C5.2 Grading & Drainage Plan.dwg Date: 4/15/2026 2:28 PM

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5550 WEST IDLEWILD AVE. SUITE 115
TAMPA, FLORIDA 33634 (813) 330-2701
CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:
RUNWAY 36 RSA GRADING

Designer: JM	Checked By: MA
Technician: JM	ICE Project Name: 26-031
Engineer of Record:	

NOTES:

REVISIONS			
NO.	DESCRIPTION	DATE	BY

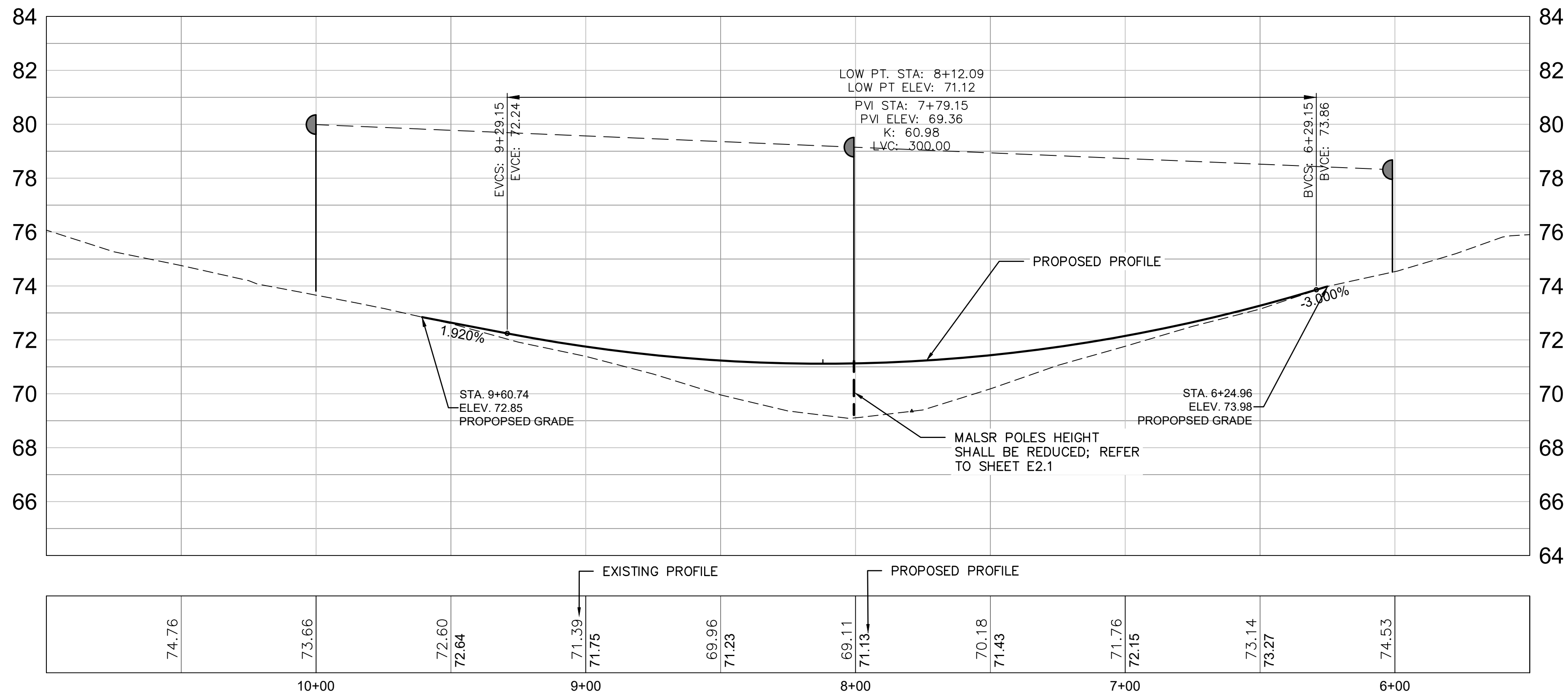
Drawing Name:
PLAN AND PROFILE

FAA A.I.P. Project No.:
341-334-000-000-09-33173

FDOT Project No.:
341-334-000-000-09-33173

Date:
APRIL 2026

Sheet Number:
C6.1



File Path: C:\Users\jmarin\adk\adk\Bowl\CEP\AVIATION\Ocala\26-031 RSA Grading\CADD\dwg\011_C6-1_C6.1_Plan and Profile.dwg Date: 4/15/2026 2:30 PM

90% DESIGN DOCUMENTS - NOT FOR CONSTRUCTION

LEGEND

- EXIST. ASPHALT PAVEMENT
- RSA — EXISTING RUNWAY SAFETY AREA
- ROFA — EXISTING RUNWAY OBJECT FREE AREA
- OFZ — EXISTING RUNWAY OBJECT FREE ZONE
- EXISTING RUNWAY LIGHTS

NOTE:

- ALL DISTURBED AREAS WITHIN THE LIMITS OF CONSTRUCTION NOT OTHERWISE IMPROVED SHALL BE SODDED AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.
- SOD SHALL BE CERTIFIED, MACHINE-CUT TURFGRASS (ARGENTINE BAHIA) MEETING APPLICABLE STATE AND PROJECT SPECIFICATIONS. SOD SHALL BE FREE OF WEEDS, PESTS, DISEASE, AND FOREIGN MATERIALS.
- ALL SOD SHALL BE ROLLED OR TAMPED IMMEDIATELY AFTER PLACEMENT TO ENSURE FIRM CONTACT WITH THE UNDERLYING SOIL AND ELIMINATE AIR POCKETS.
- SOD PLACED ON SLOPES STEEPER THAN 1V:4H SHALL BE STAKED OR OTHERWISE SECURED TO PREVENT DISPLACEMENT UNTIL ESTABLISHED. THE CONTRACTOR SHALL WATER SOD IMMEDIATELY AFTER INSTALLATION AND MAINTAIN ADEQUATE MOISTURE TO ENSURE ESTABLISHMENT. WATERING SHALL CONTINUE UNTIL THE SOD IS FULLY ROOTED AND ACCEPTED.
- THE CONTRACTOR SHALL MAINTAIN ALL SODDED AREAS UNTIL FINAL ACCEPTANCE, INCLUDING MOWING, WATERING, FERTILIZATION, AND REPLACEMENT OF ANY DEAD OR DAMAGED AREAS.
- ANY AREAS OF SOD THAT FAIL TO ESTABLISH, SHOW SIGNS OF EROSION, OR ARE DAMAGED SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL COORDINATE SODDING OPERATIONS WITH GRADING ACTIVITIES TO PREVENT EROSION AND ENSURE PROPER DRAINAGE PATTERNS ARE MAINTAINED.
- SODDING SHALL NOT BE PERFORMED DURING UNSUITABLE WEATHER CONDITIONS (E.G. HEAVY RAINFALL) UNLESS APPROVED BY THE ENGINEER.
- REFER TO THE DEMOLITION PLANS FOR THE LIMITS OF SODDING.
- NO PAVEMENT MILLING SHALL BE PERMITTED WITHIN THE PROJECT LIMITS UNLESS OTHERWISE APPROVED IN WRITING BY THE ENGINEER AND AIRPORT. ALL DISTURBED TURF AREAS SHALL BE RESTORED UPON COMPLETION OF CONSTRUCTION. FINAL TURFING SHALL CONSIST OF EITHER BERMUDA OR BAHIA GRASS

CONTROL POINT
SET IRON ROD & CAP (5/8" WITL B5091)
NORTHING = 1753964.6440
EASTING = 584574.6840
ELEVATION = 65.23'

CONTROL POINT
SET IRON ROD & CAP (5/8" WITL B5091)
NORTHING = 1753935.0300
EASTING = 585419.6290
ELEVATION = 72.37'

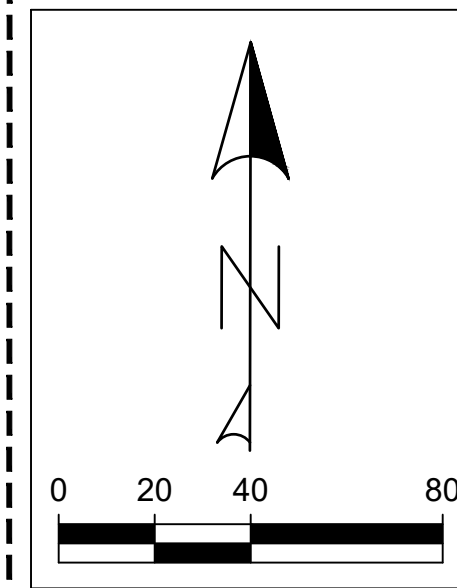
FOUND NAIL & DISK (N5-3 2024)
N=1753915.8270
E=584979.8280
ELEVATION = 72.85'

LIGHT PEDESTAL
CONCRETE ELEVATION = 74.45'
BASE PLATE ELEVATION = 74.55'
CENTER OF LIGHT ELEVATION = 78.17'

LIGHT PEDESTAL
CONCRETE ELEVATION = 69.19'
BASE PLATE ELEVATION = 70.88'
CENTER OF LIGHT ELEVATION = 78.48'

LIGHT PEDESTAL
CONCRETE ELEVATION = 74.07'
CENTER OF LIGHT ELEVATION = 79.22'
LIGHT PEDESTAL
CONCRETE ELEVATION = 74.23'
CENTER OF LIGHT ELEVATION = 79.38'

LIGHT PEDESTAL
CONCRETE ELEVATION = 74.08'
CENTER OF LIGHT ELEVATION = 79.20'



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5550 WEST IDLEWILD AVE. SUITE 115
TAMPA, FLORIDA 33634 (813) 330-2701
CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:

RUNWAY 36 RSA GRADING

Designer: JM

Checked By: MA

Technician: JM

ICE Project Name: 26-031

Engineer of Record:

NOTES:

REVISIONS

NO.	DESCRIPTION	DATE	BY

Drawing Name:

SODDING PLANS

FAA A.I.P. Project No.:

341-334-000-000-09-33173

FDOT Project No.:

341-334-000-000-09-33173

Date:

APRIL 2026

Sheet Number:

C7.1

90% DESIGN DOCUMENTS - NOT FOR CONSTRUCTION

AIRFIELD ELECTRICAL LEGEND			
EXISTING		RELOCATE(D)	PROPOSED
	EXISTING PAR-38 MALSR APPROACH BAR ON LIR SUPPORT STRUCTURE		REINSTALLED PAR-38 LIGHT MALSR APPROACH BAR ON NEW LIR SUPPORT STRUCTURE
	(CYAN SYMBOLS DENOTES SIGN TO BE REMOVED AND STORED)		
☒	EXISTING ELECTRICAL HANDHOLE.		

ABBREVIATIONS

ABC – AGGREGATE BASE COURSE	FAA – FEDERAL AVIATION ADMINISTRATION	NTS – NOT TO SCALE	S – STRAIGHT
AIP – AIRPORT IMPROVEMENT PROGRAM	FOD – FOREIGN OBJECT DEBRIS	OC – ON CENTER	SCH – SCHEDULE
AOA – AIRCRAFT OPERATIONS AREA	G – GREEN	OFA – OBJECT FREE AREA	SGN – SIGN
AWG – AMERICAN WIRE GAUGE	GRSC – GALVANIZED RIGID STEEL CONDUIT	PC – POINT OF CURVATURE	SIDA – SECURITY IDENTIFICATION DISPLAY AREA
B – BASELINE	GS – GLIDE SCOPE	PCC – PORTLAND CEMENT CONCRETE	SPT – STANDARD PENETRATION TEST
B,S,D,CU – BARE SOFT DRAWN COPPER	IE/INV – INVERT ELEVATION / INVERT	PERF – PERFORATED	SS – STAINLESS STEEL
C – CENTERLINE	ILS – INSTRUMENT LANDING SYSTEM	PG – PROPOSED GRADE	STA – STATION
C – CONDUIT	IP – INTERSECTION POINT	PT – POINT OF TANGENCY	STD – STANDARD
DB – DIRECT BURIED	JB – JUNCTION BOX	PVC – POLYVINYL CHLORIDE PIPE	TL – TAXILANE
CEDB – CONCRETE ENCASED DUCTBANK	KV – KILOVOLT	PVI – POINT OF VERTICAL INTERSECTION	TTL – TOTAL
CONC – CONCRETE	LHA – LIGHT HOUSING ASSEMBLY (PAPI)	R – RED	TW – TAXIWAY
CPP – CORRUGATED PLASTIC PIPE	LIR – LOW IMPACT RESISTANT	ROFA – RUNWAY OBJECT FREE AREA	TDZ – TOUCHDOWN ZONE
CSO – CONTRACTOR SECURITY OFFICER	LF – LINEAR FEET	RSA – RUNWAY SAFETY AREA	TSR – TAXIWAY SAFETY AREA
DEMO – DEMOLITION	LT – LEFT	RW – RUNWAY	TYP – TYPICAL
DIA – DIAMETER	MIN – MINIMUM	RCP – REINFORCED CONCRETE PIPE	UON – UNLESS OTHERWISE NOTED
DRI – DOUBLE RING INFILTRATER	N – NOTHING	RGL – RUNWAY GUARD LIGHT	W – WHITE
DWG – DRAWING	NAD – NORTH AMERICAN DATUM	RPZ – RUNWAY PROTECTION ZONE	Y – YELLOW
E – EASTING	NGVD – NATIONAL GEODETIC VERTICAL DATUM	RSA – RUNWAY SAFETY AREA	
EG – EXISTING GROUND	NOTAM – NOTICE TO AIRMEN	RT – RIGHT	
EL/ELEV – ELEVATION			
EOP – EDGE OF PAVEMENT			
EQ – EQUAL			
ERCP – ELLIPTICAL REINFORCED CONCRETE PIPE			
ERSA – EXTENDED RUNWAY SAFETY AREA			
EX/EXST/EXIST – EXISTING			

GENERAL ELECTRICAL NOTES

1. THE CONTRACTOR SHALL PROVIDE ALL MATERIALS AND LABOR TO RELOCATE, MODIFY AND INSTALL THE AIRFIELD ELECTRICAL SYSTEMS AS INDICATED ON THE DRAWINGS. ITEMS NOT SHOWN BUT OBVIOUSLY NECESSARY FOR COMPLETION OF THE WORK SHALL BE INCLUDED.
2. NEW MATERIALS SHALL BE U.L. APPROVED.
3. ALL WORK SHOWN TO BE DEMOLISHED ON THE DRAWINGS IS BASED ON FIELD OBSERVATIONS OF THE ACTUAL EXISTING CONDITIONS AND ON EXISTING "AS-BUILT" DRAWINGS OF THE AREAS AFFECTED. THEY ARE THEREFORE CONSIDERED TO BE SCHEMATIC. IT IS THE INTENT OF THE DEMOLITION DRAWINGS THAT ALL EQUIPMENT, DEVICES, FIXTURES, WIRING MATERIALS, SYSTEM AND APPURTENANCES, ETC. WHICH ARE NO LONGER REQUIRED AS A RESULT OF THE PROJECT TO BE REMOVED. THE OWNER HAS FIRST RIGHT OF REFUSAL FOR ALL REMOVED ITEMS.
4. ELECTRICAL DEMOLITION WORK SHALL BE LIMITED TO THE AREAS AND SCHEDULES INDICATED.
5. SHOULD ANY RUNWAY OR TAXIWAY SYSTEM BE INOPERABLE DUE TO THE CONTRACTOR'S WORK, AND THE CONTRACTOR IS UNABLE TO RESTORE THE SYSTEM BY NIGHTFALL WITH PERMANENT REPAIRS, THE CONTRACTOR SHALL AT HIS OWN EXPENSE TAKE NECESSARY MEASURES TO INSURE OPERATIONS OF THE SYSTEM DURING NIGHT HOURS. TEMPORARY WORK SHALL BE SUBJECT TO THE ENGINEER'S APPROVAL. IF THE SYSTEM CANNOT BE RESTORED BY NIGHTFALL, THE CONTRACTOR SHALL INSTALL A TEMPORARY SYSTEM OF BATTERY OPERATED LIGHTS WITH THE APPROPRIATE COLORED LENSES FOR BOTH.
6. ITEMS SHOWN IN SCREEN (GHOST) ARE EXISTING ITEMS AND ITEMS SHOWN IN SOLID (BOLD) ARE NEW AND TO BE PROVIDED UNDER THIS CONTRACT UNLESS OTHERWISE NOTED.
7. PROJECT PAY ITEMS: THE PROJECT PAY ITEMS ARE PROVIDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL WORK TO BE IDENTIFIED WITH A SPECIFIC PAY ITEMS IS TO BE CONSIDERED REQUIRED WORK TO COMPETE THE PROJECT AND IS TO BE SUBSIDIARY TO THE COST OF PROJECT PAY ITEMS PROVIDED.
8. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO STARTING WORK.
9. OPERATING TEST

UPON COMPLETION OF TESTS, SHOW BY DEMONSTRATION IN SERVICE THAT CIRCUITS, CONTROL EQUIPMENT, AND LIGHTS COVERED BY THE CONTRACT ARE IN GOOD OPERATING CONDITION. OPERATE EACH SWITCH IN THE LIGHTING PANELS SO THAT EACH SWITCH POSITION IS ENGAGED AT LEAST TWICE. DURING THIS PROCESS, OBSERVE LIGHTS AND ASSOCIATED EQUIPMENT TO DETERMINE THAT EACH SWITCH CONTROLS PROPERLY CORRESPONDING CIRCUIT. PROVIDE TELEPHONE OR RADIO COMMUNICATION BETWEEN THE OPERATOR AND THE OBSERVERS. REPEAT TESTS FROM THE ALTERNATE CONTROL STATION, FROM THE REMOTE CONTROL POINTS, AND AGAIN FROM THE LOCAL CONTROL SWITCHES ON THE REGULATORS. TEST EACH LIGHTING CIRCUIT BY OPERATING THE LAMPS AT MAXIMUM BRIGHTNESS FOR NOT LESS THAN 30 MINUTES. VISUALLY EXAMINE AT THE BEGINNING AND AT THE END OF THIS TEST TO ENSURE THAT THE CORRECT NUMBER OF LIGHTS ARE BURNING AT FULL BRIGHTNESS. CONDUCT ONE DAY AND ONE NIGHT OPERATING TEST FOR THE ENGINEER.

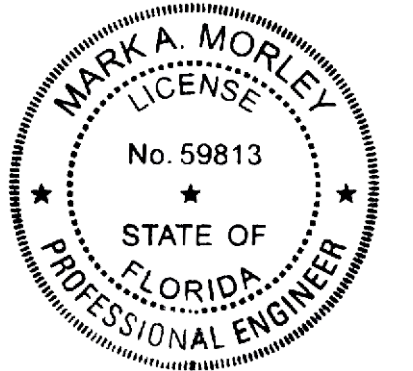


Project Name:

**R/W 36 RSA
GRADING**

Designer: BPR	Checked by: MAM
Technician: BPR	TOG Proj. No.: 00892-01-0126

Engineer of Record:



MARK A. MORLEY, P.E. NO. 59813



REVISIONS

No.	Description	Date	By

Drawing Name:
MALSR AIRFIELD ELECTRICAL LEGEND AND NOTES

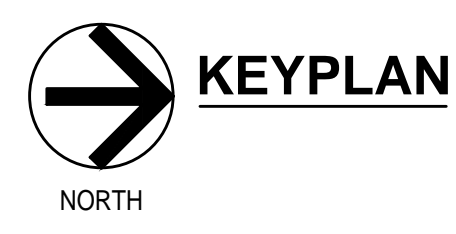
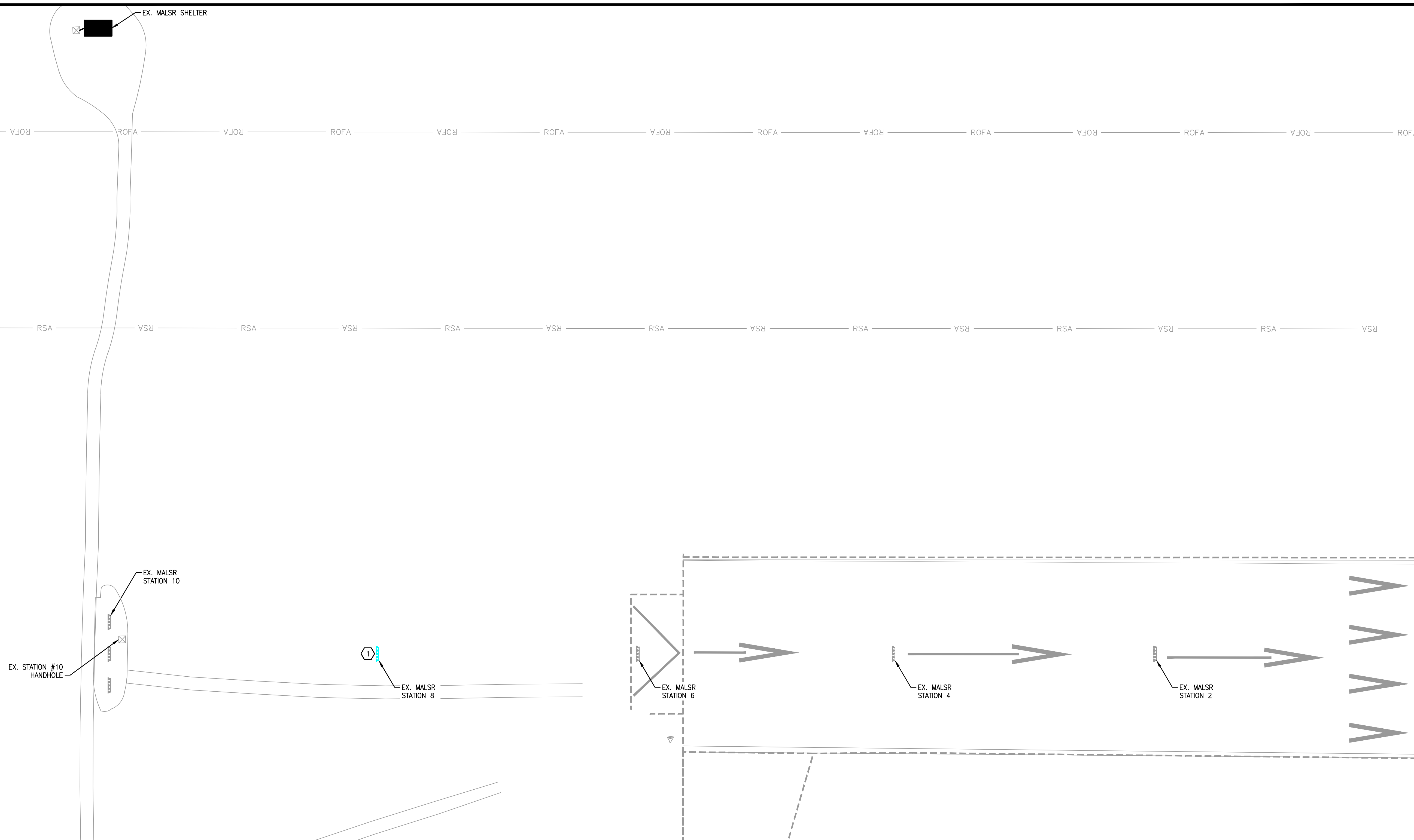
FDOT Funding No.: 341-334-000-000-09-33173	FAA Funding No.: 341-331-000-000-09-33173
Date: APRIL 2026	PRELIMINARY
AS SHOWN	Sheet Number: E0.1



SHEET LIST

Sheet Number	Sheet Title
E0.1	MALSR Airfield Electrical Legend and Notes
E1.1	MALSR Modification Plan
E2.1	MALSR Profile Plan
E3.0	MALSR Station 08 Details
E3.1	MALSR Station 08 Assembly Details
E3.2	MALSR Station 08 Foundation & Grounding Details

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IE
INFRASTRUCTURE
 CONSULTING & ENGINEERING
 5550 WEST IDLEWILD AVE. SUITE 102
 TAMPA, FLORIDA 33634 (813) 330-2701
 CERTIFICATE OF AUTHORIZATION NO.: 30862

Project Name:
R/W 36 RSA GRADING

Designer: BPR	Checked by: MAM
Technician: BPR	TOG Proj. No.: 00892-01-0126

Engineer of Record:

MARK A. MORLEY, P.E. NO. 59813



REVISIONS			
No.	Description	Date	By

Drawing Name:
MALSR MODIFICATION PLAN

FDOT Funding No.: 341-334-000-000-09-33173	FAA Funding No.: 341-331-000-000-09-33173
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Date: APRIL 2026	PRELIMINARY
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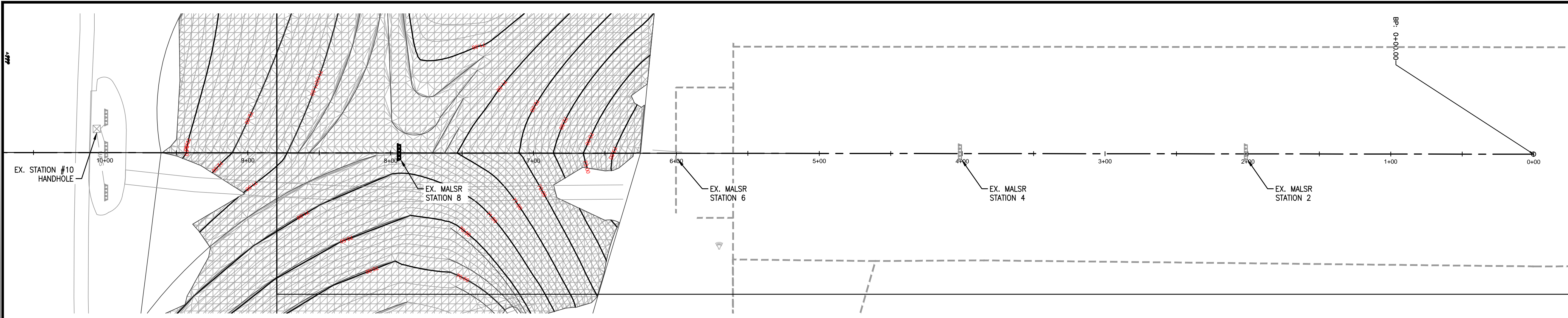
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GENERAL NOTE **KEYED NOTE**

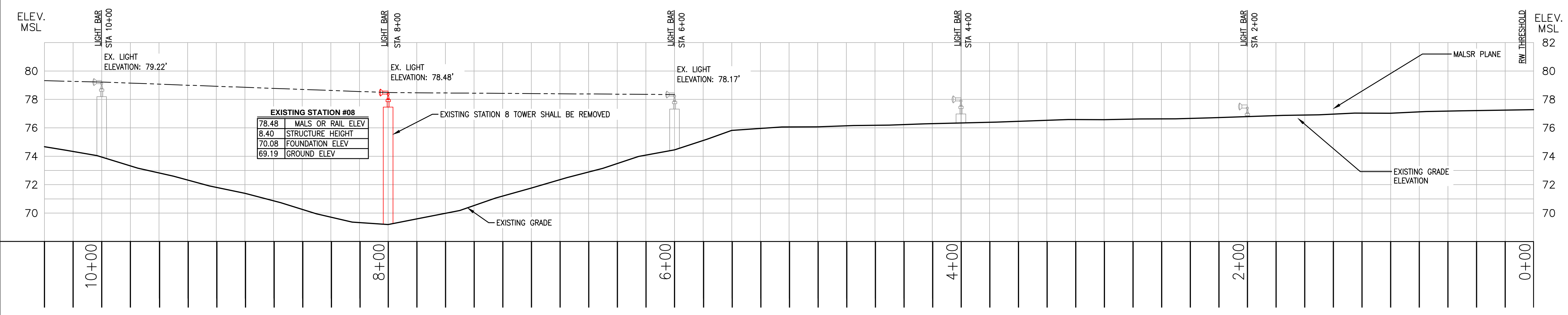
1. SEE E0.1 FOR GENERAL NOTES. ① CAREFULLY REMOVE THE EXISTING MALSR STATION #08 TOWER, AND PREPARE FOR REINSTALLATION AT THE NEW GRADE. RETAIN AND PROTECT THE LIGHT FIXTURE AND LIGHT BAR ASSEMBLY. COMPLETELY REMOVE AND DISPOSE OF THE LIR POST, BASE CAN AND FOUNDATION OFF-SITE. MOUNTING STAND TO BE REPLACED WITH A NEW ONE. ENSURE ALL MATERIALS ARE DISPOSED OF IN COMPLIANCE WITH ENVIRONMENTAL REGULATIONS.

PRELIMINARY SUBMITTAL

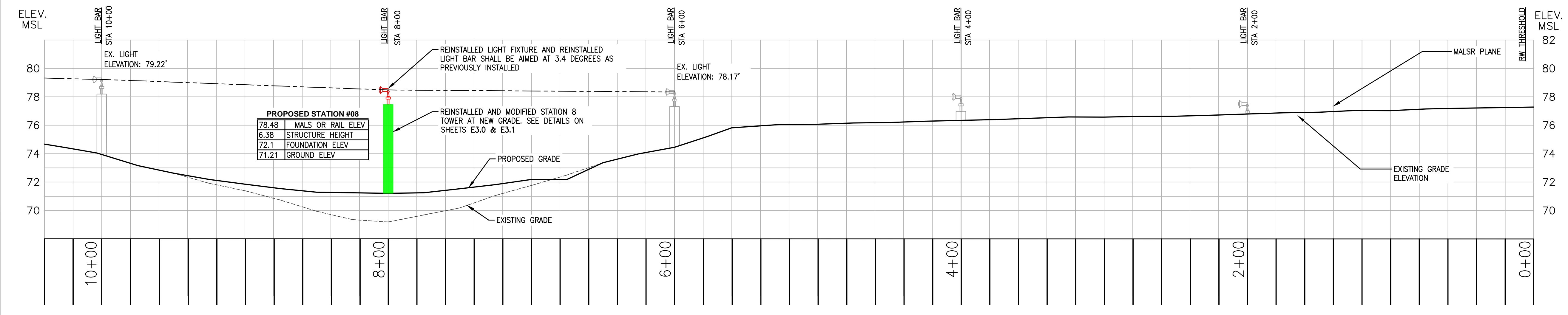
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MALS GROUND ELEVATION REFERENCE PLAN
 NORTH
 0 20 40 80 120 SCALE: 1" = 40'-0"



MALS PROFILE PLAN - EXISTING
 NORTH
 0 20 40 80 120 SCALE: 1" = 40'-0"



MALS PROFILE PLAN - PROPOSED
 NORTH
 0 20 40 80 120 SCALE: 1" = 40'-0"

GENERAL NOTE

1. SEE E0.1 FOR GENERAL NOTES.



Project Name:
R/W 36 RSA GRADING
 Designer: BPR
 Checked by: MAM
 Technician: BPR
 TOG Proj. No.: 00892-01-0126
 Engineer of Record:



MARK A. MORLEY, P.E. NO. 59813



REVISIONS

No.	Description	Date	By

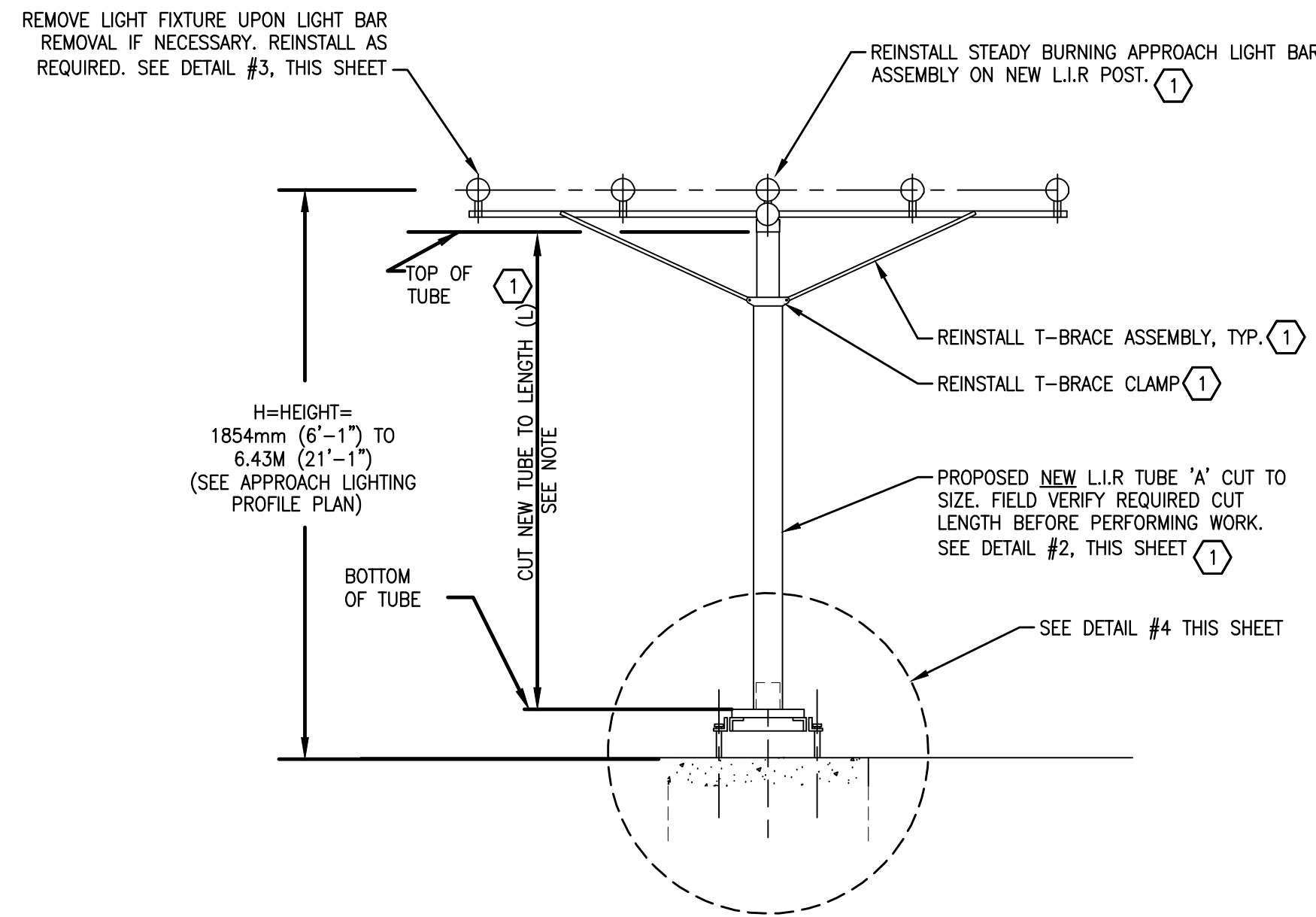
Drawing Name:
MALS PROFILE PLAN

FDOT Funding No.: 341-334-000-000-09-33173	FAA Funding No.: 341-331-000-000-09-33173
Date: APRIL 2026	PRELIMINARY
AS SHOWN	Sheet Number: E2.1

PRELIMINARY SUBMITTAL

NOTES

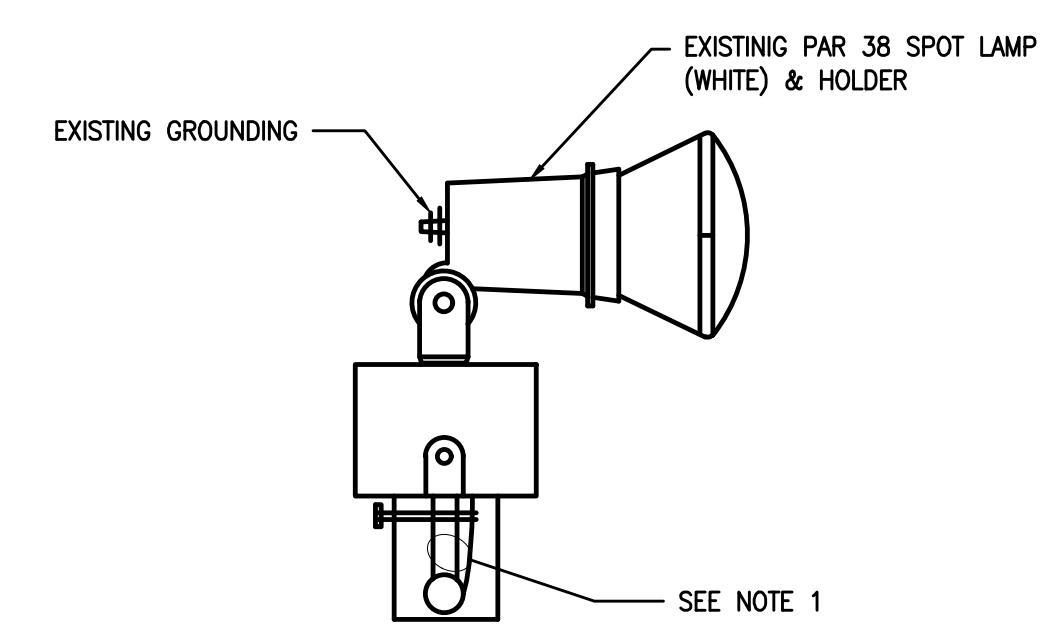
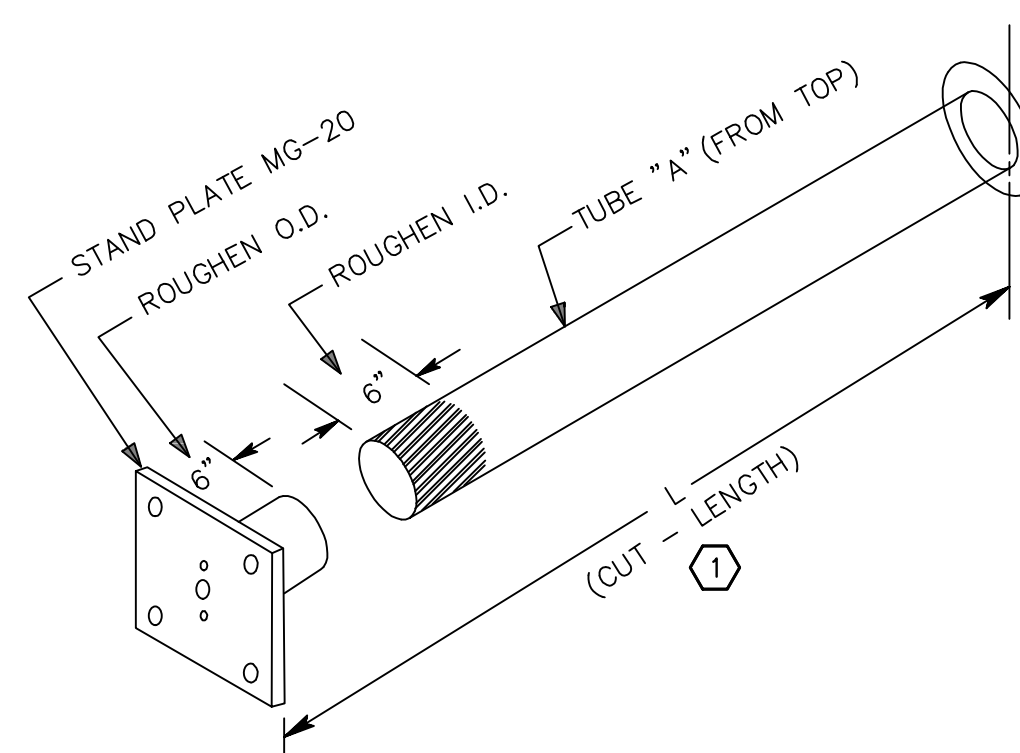
1. THE LOW-IMPACT RESISTANT (LIR) STRUCTURE MG-20 SHOWN HEREIN IS TO BE USED TO SUPPORT LIGHTS OF AN APPROACH LIGHTING SYSTEM IN A FIXED ALIGNMENT AND ORIENTATION AT HEIGHTS FROM 6'-1" TO 40'-0".
5. LIR STRUCTURES MANUFACTURED BY JAQUITH INDUSTRIES INC, SYRACUSE, NY., OR CK COMPOSITES INC, MT PLEASANT, PA.
6. FOR MG-20, THE CUT LENGTH L EQUATION BECOMES: $L=H-(V-6 \frac{3}{4})$ EITHER TUBE "A" OR THE PORTION ABOVE THE HOOP WOUND RING OF TUBE "B" MAY BE USED.
7. A LENGTH OF TUBE SPLICE, WHICH FITS THE ID OF TUBE "B", IS LOCATED 60 INCHES FROM ONE END AND IS FACTORY BONDED TO THE END WHICH IS CLOSEST TO THE HOOP WOUND RING. THIS SPLICE IS INSERTED 4 INCHES INTO TUBE "B" AND HAS 4 INCHES OF ROUGHENED SURFACE PROTRUDING BEYOND THE END. AT LEAST 4 INCHES OF THE INSIDE OF TUBE "A" IS ROUGHENED TO MATCH.



NOTES:
 LENGTH OF TUBE SHALL BE DETERMINED AS FOLLOWS:
 $L = H - V - 171.5\text{mm (6.75")}$
 WHERE:
 L = LENGTH OF TUBE REQUIRED
 H = LIGHT MOUNTING HEIGHT (AS DETERMINED BY FIELD SURVEY AND PROFILE PLAN)
 V = DISTANCE FROM CENTERLINE OF LIGHT FIXTURE TO TOP OF FIBERGLASS TUBE. THIS DISTANCE MUST BE DETERMINED USING ACTUAL HARDWARE TO BE INSTALLED.

KEYED NOTE

- ① CONTRACTOR TO INSTALL/REINSTALL EQUIPMENT PER MANUFACTURER'S RECOMMENDATION

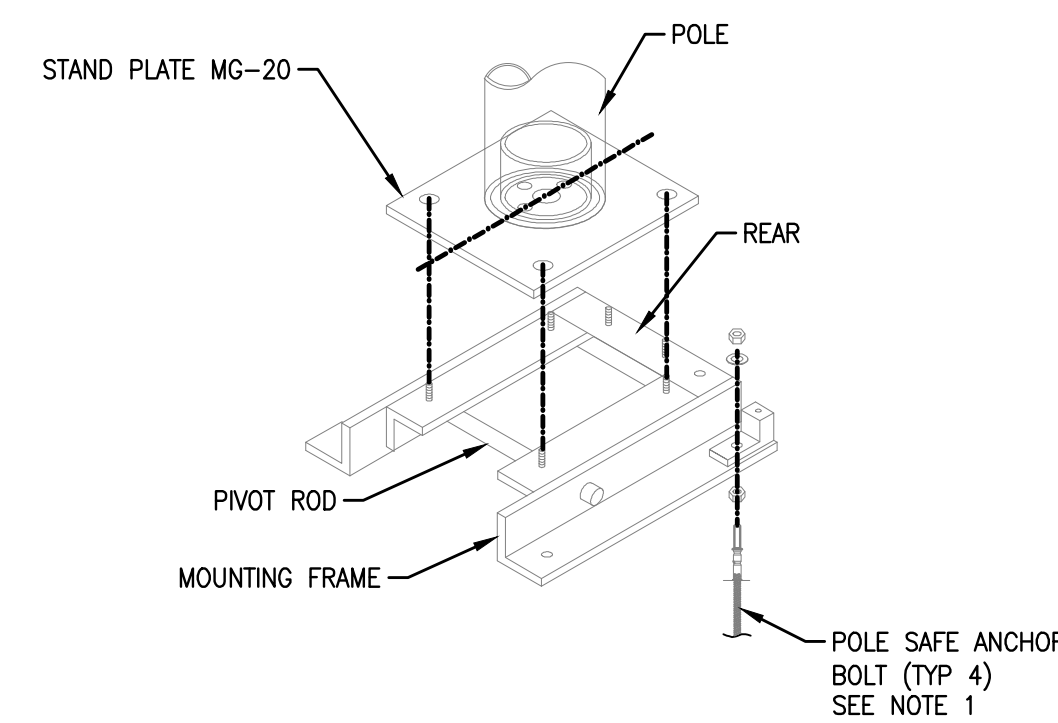
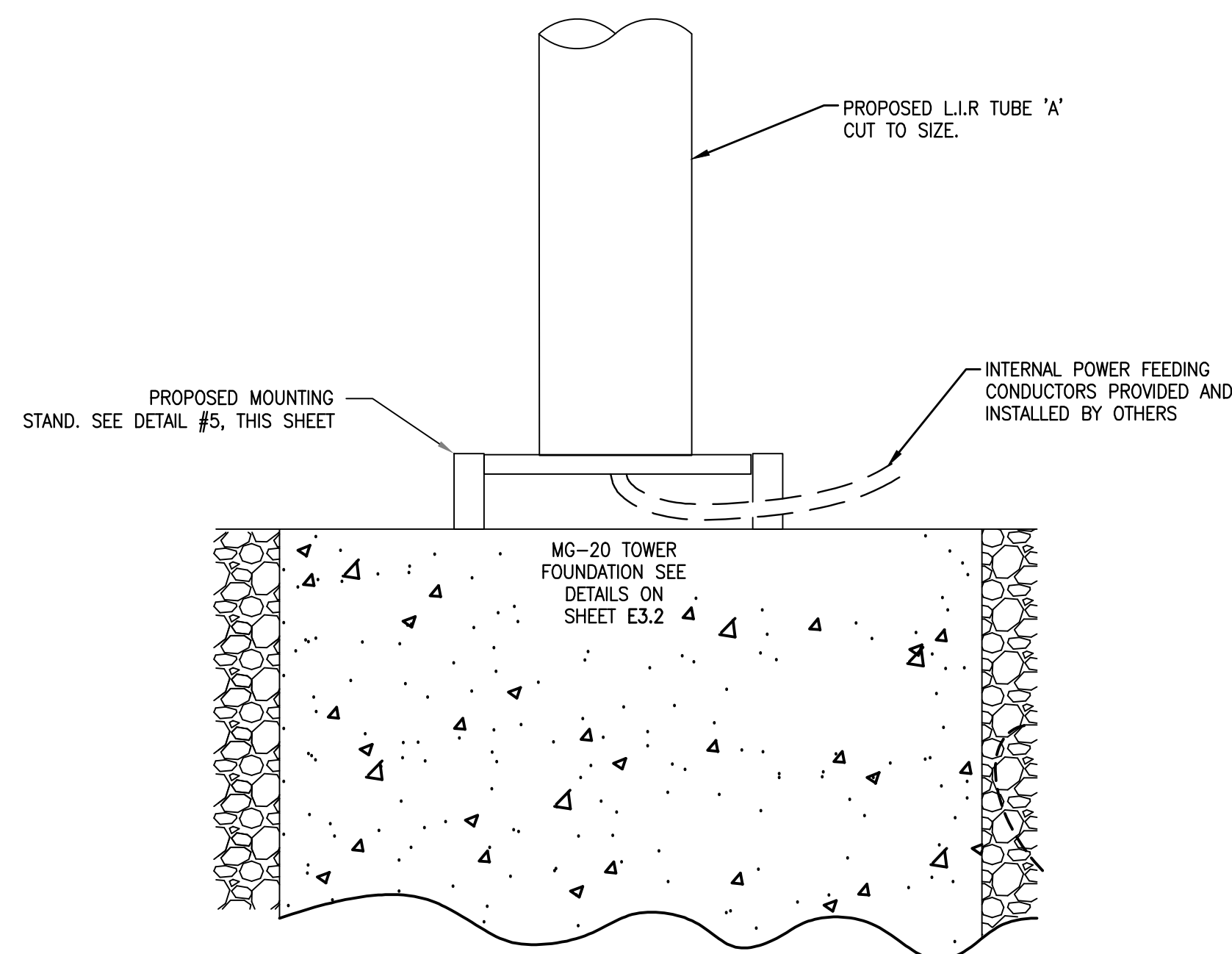


- NOTE**
 1. REUSE EXISTING CONDUCTORS SERVING PREVIOUS INSTALLATION

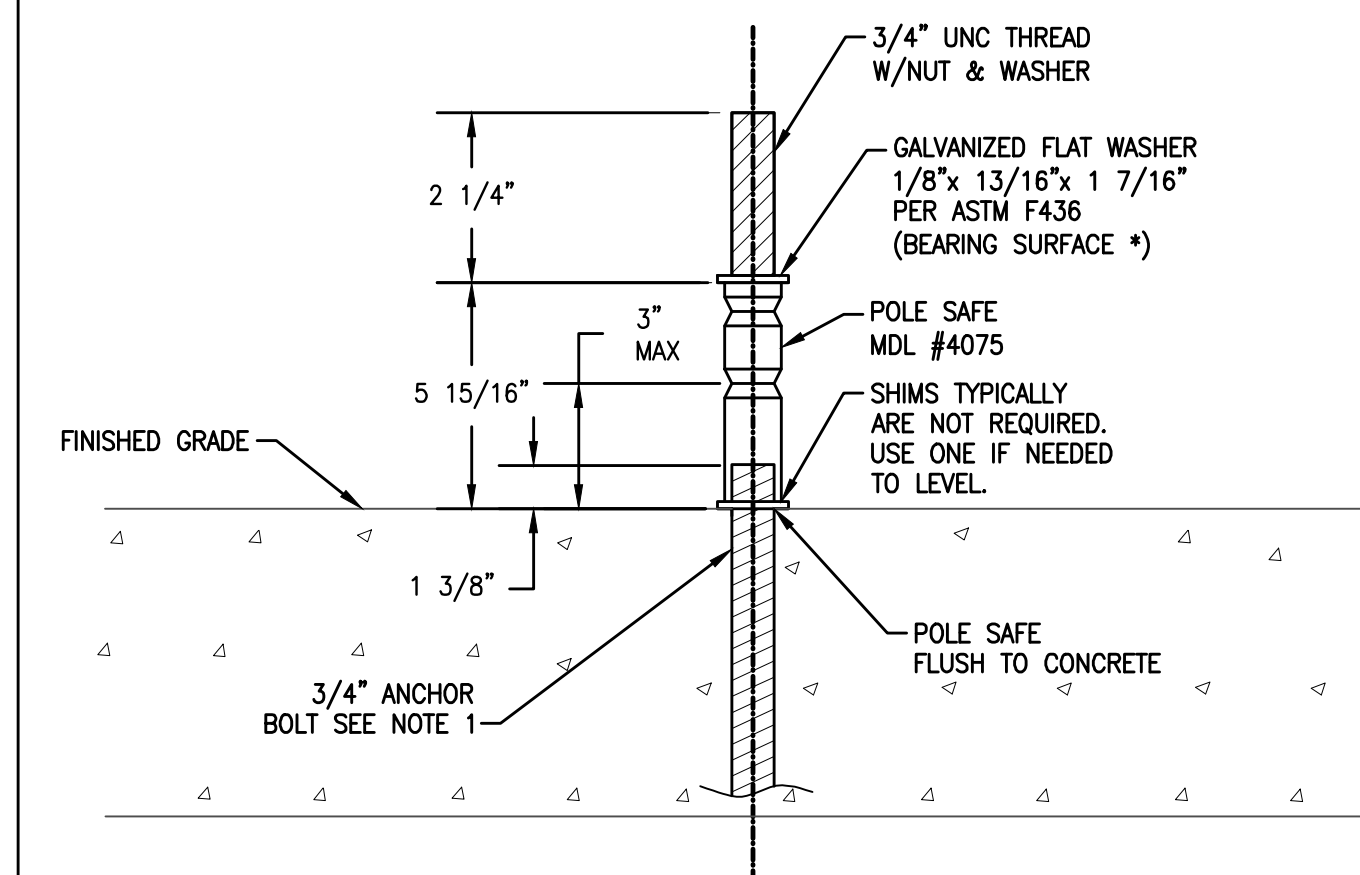
1 STATION 08 LOW-IMPACT-RESISTANT STRUCTURE MG-20
 SCALE: NOT TO SCALE

2 MG-20 MAST ASSEMBLY
 SCALE: NOT TO SCALE

3 LAMPHOLDER
 SCALE: NOT TO SCALE



- NOTE**
 1. FOR MG-20 STATION 8 WHICH IS INSIDE THE RSA, THE ANCHOR BOLTS ARE TO PROJECT ABOVE THE FOUNDATION BY 1 3/8" AND THE MG MOUNTING STAND IS TO BE MOUNTED ON POLE SAFE BOLTS AS SHOWN IN DETAIL 6, THIS SHEET, WHICH WILL BE GOVERNMENT FURNISHED MATERIAL (GFM).



6 POLE SAFE - ANCHOR BOLT - (MODEL #4075 (GFM))
 SCALE: NOT TO SCALE

4 STATION 08 DETAIL
 SCALE: NOT TO SCALE

5 MOUNTING STAND ASSEMBLY
 SCALE: NOT TO SCALE



R/W 36 RSA GRADING

Designer: BPR
 Checked by: MAM
 Technician: BPR
 TOG Proj. No.: 00892-01-0126



MARK A. MORLEY, P.E. NO. 59813



REVISIONS

No.	Description	Date	By

MALSr STATION 08 DETAILS

FDOT Funding No.: 341-334-000-000-09-33173	FAA Funding No.: 341-331-000-000-09-33173
Date: APRIL 2026	PRELIMINARY
AS SHOWN	Sheet Number: E3.0



MARK A. MORLEY, P.E. NO. 59813

REVISIONS

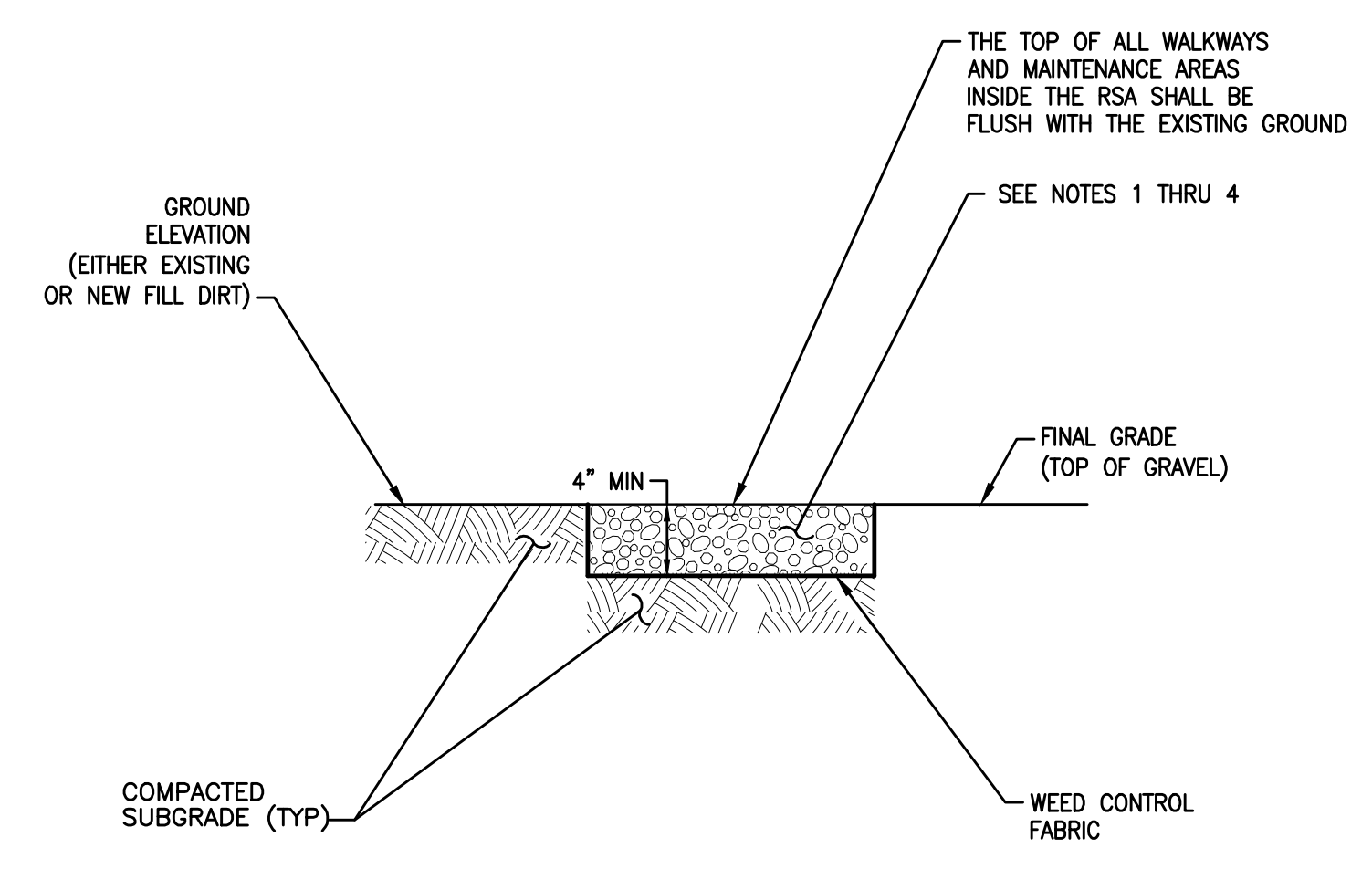
No.	Description	Date	By

Drawing Name:
MALSR STATION 08 ASSEMBLY DETAILS

FDOT Funding No.: 341-334-000-000-09-33173
FAA Funding No.: 341-331-000-000-09-33173

Date: **APRIL 2026** PRELIMINARY

AS SHOWN Sheet Number: **E3.1**

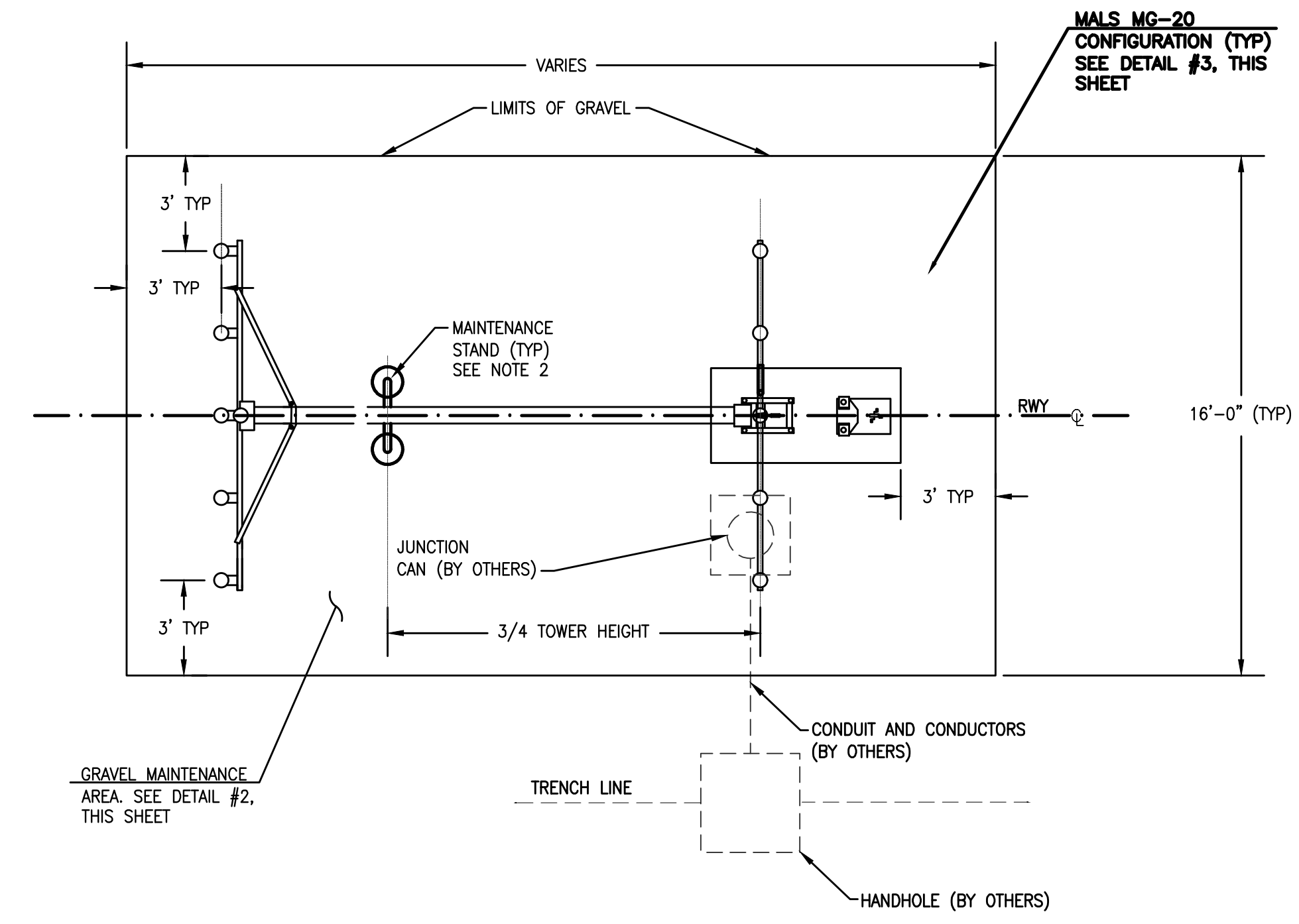


NOTES

- THE AGGREGATE SURFACE COURSE FOR THE GRAVEL ROADS AND TURNAROUND AREAS SHALL BE COMPOSED OF CRUSHED OR PARTIALLY CRUSHED COARSE AGGREGATE BONDED WITH CLEAN GRANULAR MATERIAL OR FINE AGGREGATE, OR BOTH. AGGREGATE SHALL COMPLY WITH STATE DOT SPECIFICATIONS AND SHALL BE SIMILAR TO THE FOLLOWING GRADING REQUIREMENTS WHEN TESTED IN ACCORDANCE WITH ASTM C 117 AND C 136:

A. SIZE	PERCENT BY WEIGHT PASSING SIEVES
1/2 INCH	100
3/8 INCH	85 TO 100
NO. 4	10 TO 40
NO. 8	0 TO 10
NO.16	0 TO 5

 - THE FINAL GRADATIONS DECIDED ON WITHIN THE LIMITS DESIGNATED IN THE ABOVE TABLE SHALL BE WELL-GRADED FROM COARSE TO FINE AND SHALL NOT VARY FROM THE LOW LIMIT ON ONE SIEVE TO THE HIGH LIMIT ON ADJACENT SIEVES, OR VICE-VERSA.
 - THE AMOUNT OF THE FRACTION OF MATERIAL PASSING THE NO.16 SIEVE SHALL NOT EXCEED ONE-HALF THE FRACTION PASSING THE NO.8 SIEVE.
 - THE SELECTION OF ANY OF THE GRADATIONS SHOWN IN THE ABOVE TABLE SHALL BE SUCH THAT THE MAXIMUM SIZE AGGREGATE USED IN THE AGGREGATE COURSE SHALL BE NOT MORE THAN TWO-THIRDS THE THICKNESS OF THE COURSE BEING CONSTRUCTED.
 - THE PORTION OF THE FILLER AND BINDER, INCLUDING ANY BLENDED MATERIAL, PASSING THE NO.8 SIEVE SHALL HAVE A LIQUID LIMIT NOT MORE THAN 25, AND A PLASTICITY INDEX NOT MORE THAN 6 WHEN TESTED IN ACCORDANCE WITH ASTM D 4318.
- PRIOR TO AGGREGATE INSTALLATION, A MINIMUM 3 INCHES OF TOP SOIL SHALL BE STRIPPED OF DELETERIOUS MATERIAL, LOOSENED AND HARROWED, AND THEN COMPACTED TO 95 PERCENT MODIFIED PROCTOR DENSITY AT NEAR OPTIMUM MOISTURE CONTENT IN ACCORDANCE TO ASTM D 1557. COMPACTED SOIL SHALL NOT BE PLACED IN GREATER THAN 6" LIFTS BEFORE ROLLING AND TAMPING.
- PRIOR TO AGGREGATE INSTALLATION, APPLY A WATER SOLUBLE SOIL STERILANT, THEN INSTALL A GEOTEXTILE FABRIC MARAFI 500X WITHIN THE ENTIRE ACCESS ROAD, WALKWAY AND MAINTENANCE AREAS. AGGREGATE SHALL BE GRADED TO SLOPE A MINIMUM OF 1/8" PER FOOT (1%) AWAY FROM STRUCTURES.
- BOTH AGGREGATE COURSE (BASE OR SURFACE, AS APPLICABLE) AND SUBGRADE SHALL BE COMPACTED TO 95% MODIFIED PROCTOR DENSITY AT NEAR OPTIMUM MOISTURE CONTENT IN ACCORDANCE WITH ASTM D 1557.



NOTES

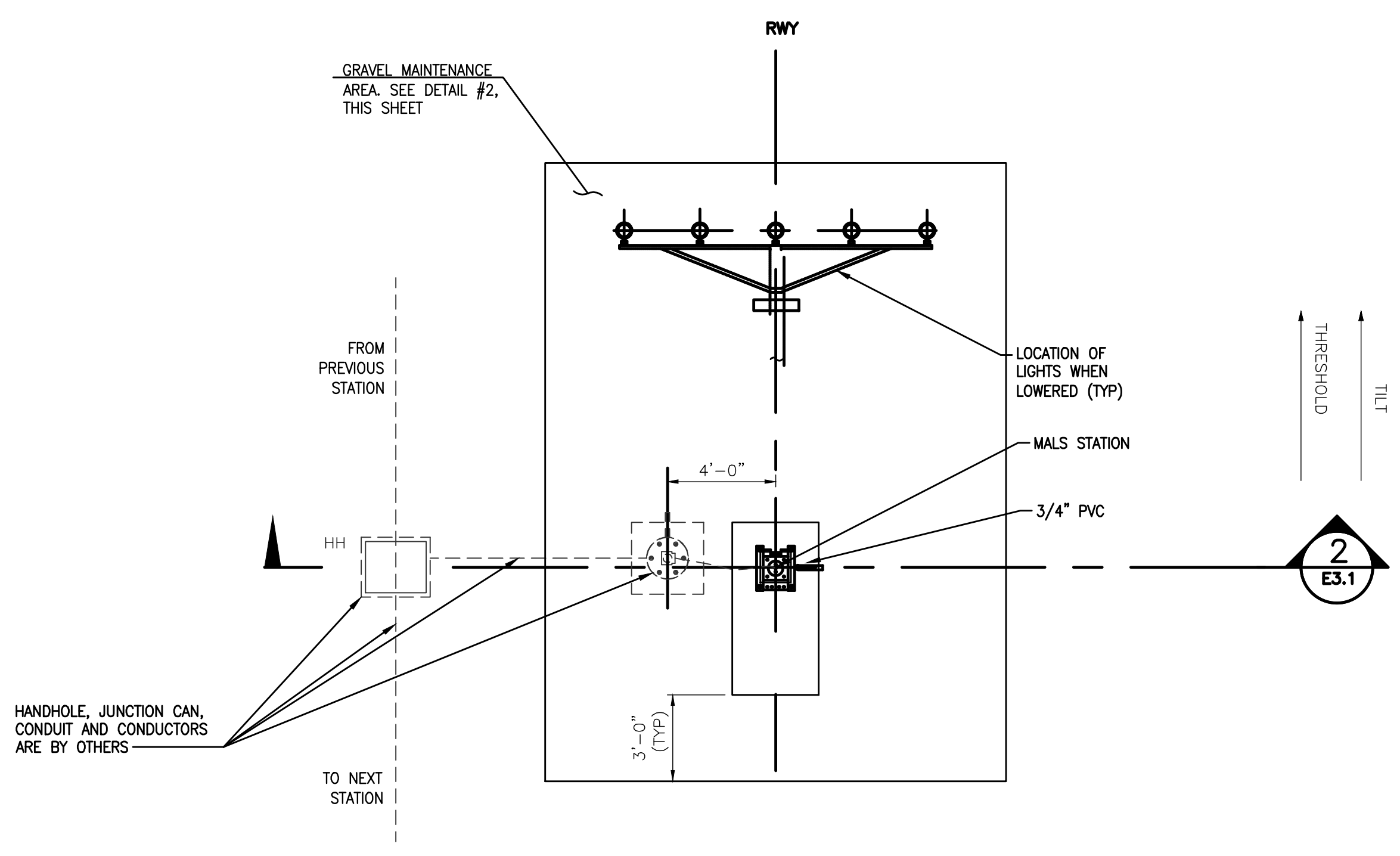
- AS APPROPRIATE, ALL MG LIR TOWERS SHALL BE LOWERED TOWARDS THE THRESHOLD FOR MAINTENANCE.
- A PORTABLE MAINTENANCE STAND SHALL BE PROVIDED FOR THE MG LIR LIGHT STATIONS WITHIN THE RSA. PORTABLE MAINTENANCE STAND SHALL BE FIBERGLASS SAWHORSE.

1 MAINTENANCE PAD LAYOUT FOR MG-20 STATION 8

SCALE: NOT TO SCALE

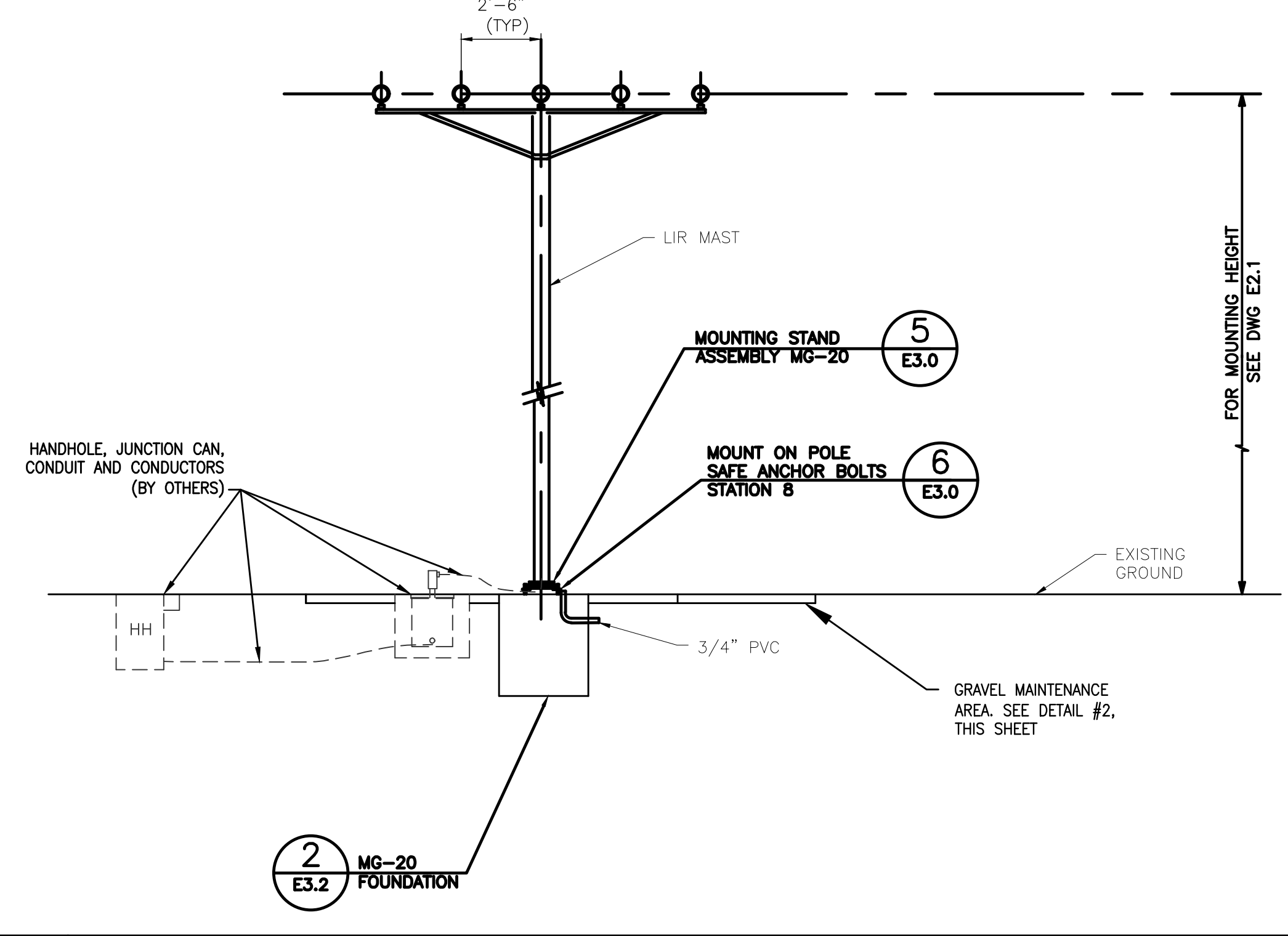
2 MAINTENANCE AREA DETAIL FOR MG-20 STATION 8

SCALE: NOT TO SCALE



3 MAINTENANCE PAD LAYOUT FOR MG-20 STATION 8

SCALE: NOT TO SCALE

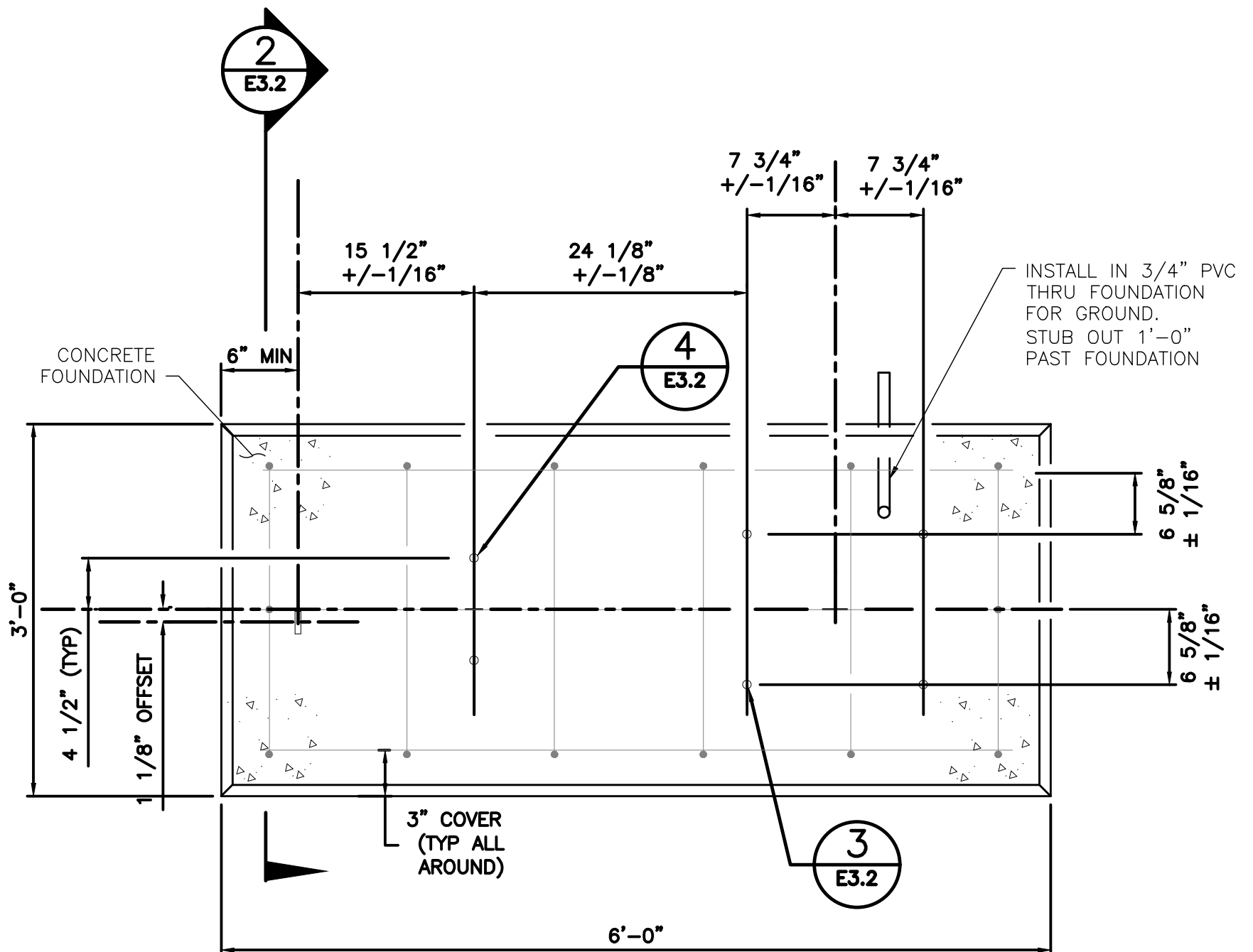


4 MAINTENANCE AREA DETAIL FOR MG-20 STATION 8

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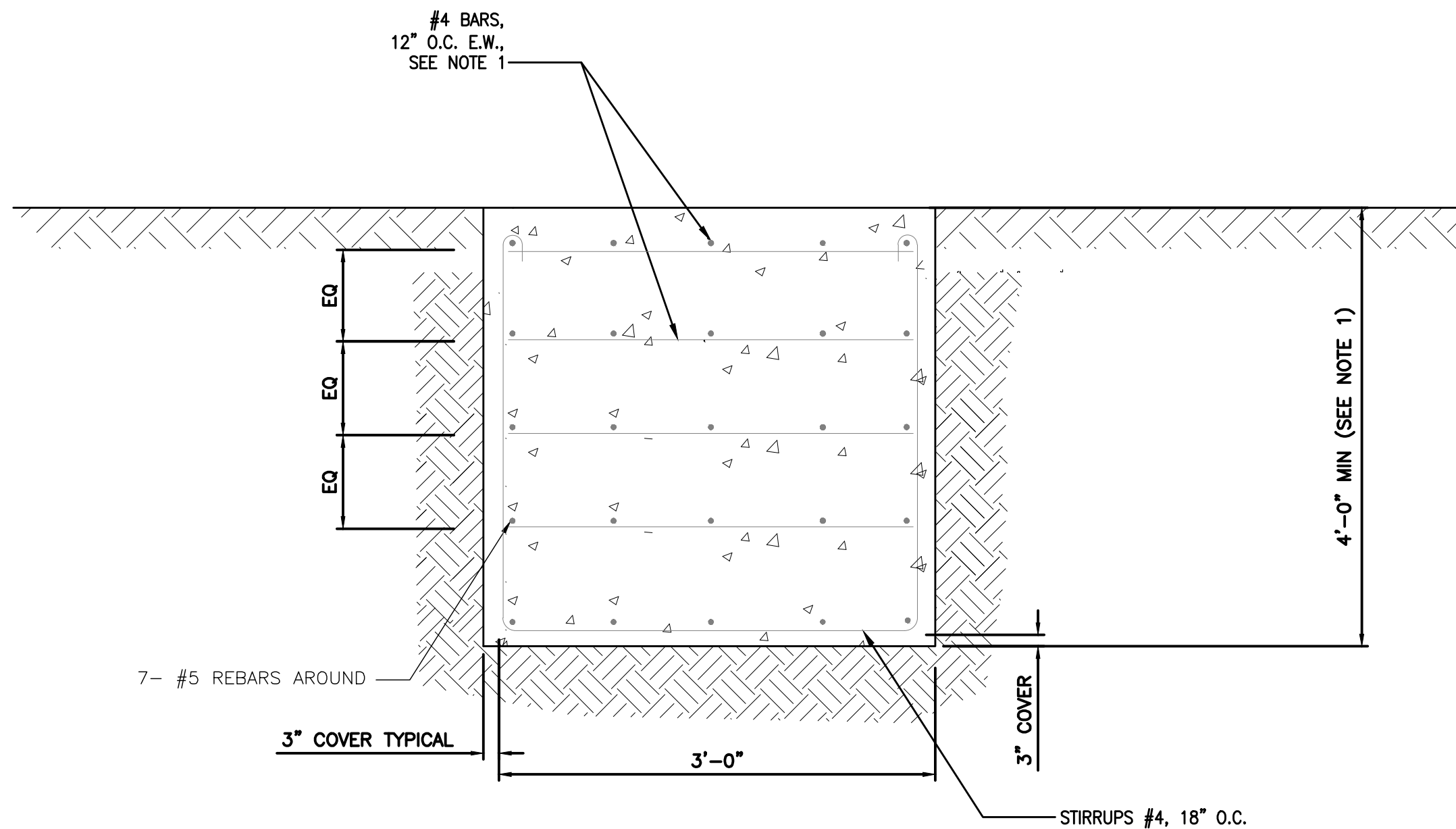
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PRELIMINARY SUBMITTAL



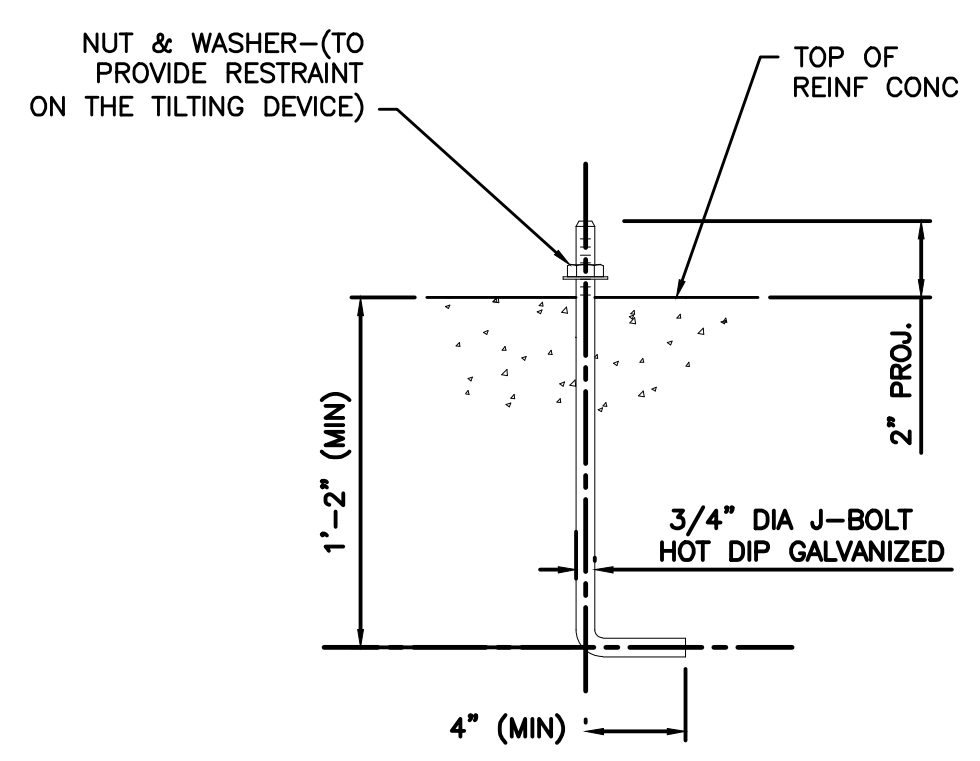
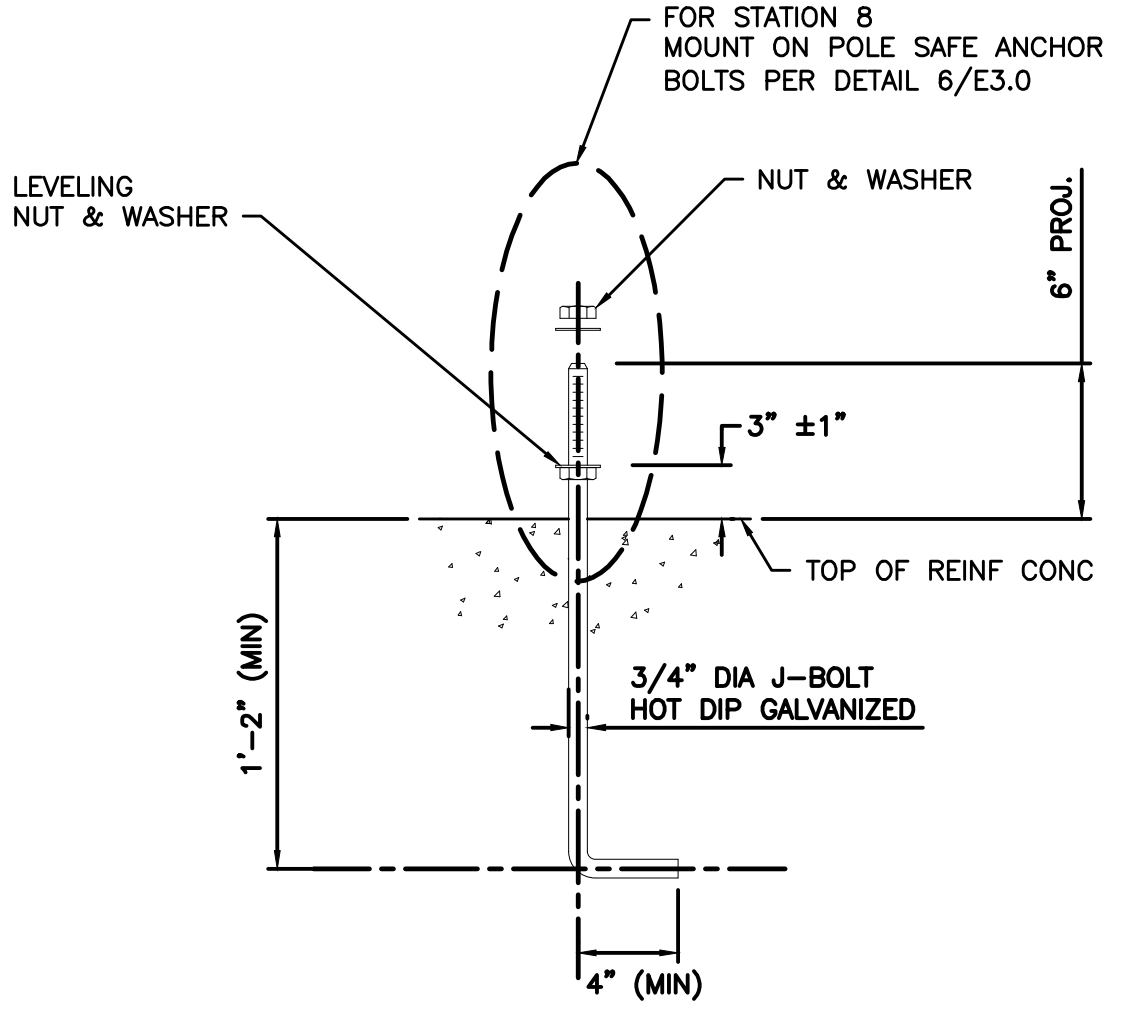
NOTES

1. ALL CONCRETE SHALL BE 3000 PSI, 28 DAY COMPRESSIVE STRENGTH, ALL REINFORCING STEEL BARS SHALL BE ASTM A615, GRADE 60. CONCRETE FOUNDATION TO BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE PARAGRAPHS OF DIVISION 03301 OF PROJECT SPECIFICATIONS.



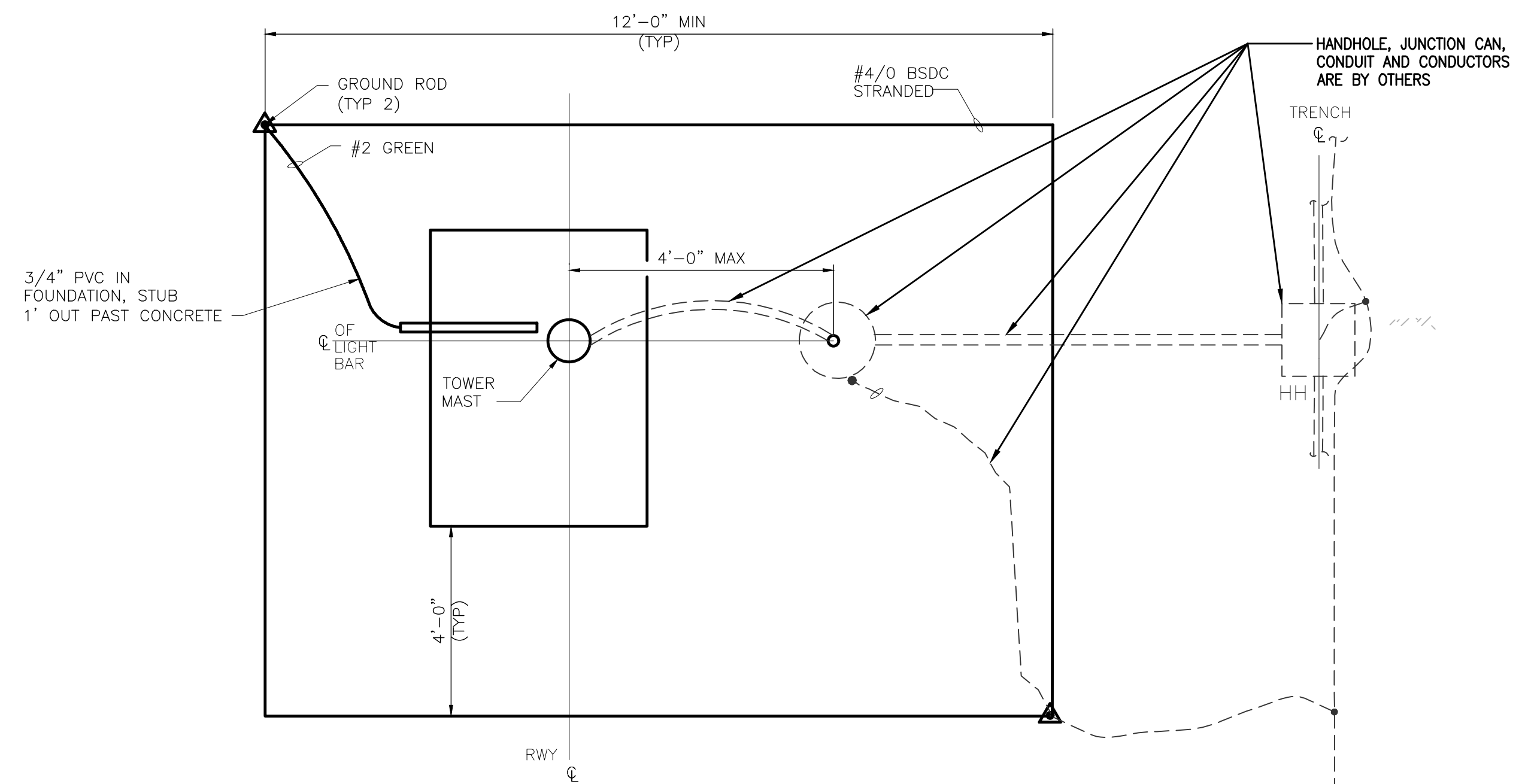
1 MAINTENANCE PAD LAYOUT FOR MG-20 STATION 8
SCALE: NOT TO SCALE

2 MAINTENANCE AREA DETAIL FOR MG-20 STATION 8
SCALE: NOT TO SCALE



3 LIR TOWER BOLTS
SCALE: NOT TO SCALE

4 ANCHOR PLATE BOLT
SCALE: NOT TO SCALE



5 MG-20 STATION GROUNDING PLAN
SCALE: NOT TO SCALE

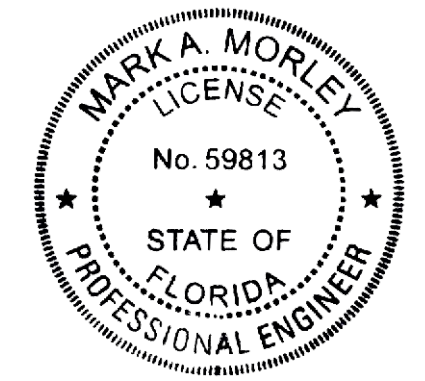


Project Name:
R/W 36 RSA GRADING

Designer: **BPR** Checked by: **MAM**

Technician: **BPR** TOG Proj. No.: **00892-01-0126**

Engineer of Record:



MARK A. MORLEY, P.E. NO. 59813



REVISIONS

No.	Description	Date	By

Drawing Name:
MALSR STATION 08 FOUNDATION & GROUNDING DETAILS

FDOT Funding No.: **341-334-000-000-09-33173** FAA Funding No.: **341-331-000-000-09-33173**

Date: **APRIL 2026** PRELIMINARY

AS SHOWN Sheet Number: **E3.2**

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PRELIMINARY SUBMITTAL